



## AGENDA

Planning Commission Meeting  
Tuesday, February 25, 2020  
7:00 pm  
Gardner City Hall  
120 E. Main Street

### CALL TO ORDER

### PLEDGE OF ALLEGIANCE

### ROLL CALL

### CONSENT AGENDA

All matters listed within the Consent Agenda have been distributed to each member of the Planning Commission for study. These items are considered to be routine and will be enacted upon by one motion with no separate discussion. If separate discussion is desired on an item, from either the Planning Commission or from the floor, that item may be removed from the Consent Agenda and placed on the Regular Agenda.

1. Standing approval of the minutes as written for the meeting on January 28, 2020.

### REGULAR AGENDA

1. **NEW LIFE COMMUNITY CHURCH**

**FDP-20-01:** Consider a final development plan for New Life Community Church located east of the intersection of S Gardner Road and E 188<sup>th</sup> Street.

2. **PRAIRIE TRACE \*\*DEFERRED TO MARCH 24, 2020\*\***

Located southeast corner of W 175<sup>th</sup> Street and Interstate 35

- a. **Z-20-01(PDP-20-01):** Hold a public hearing and consider a rezoning of approximately 136.3 acres from County RUR, PRB2, and PEC3 Districts to City of Gardner Districts RP-1 (approximately 88.2 acres) and RP-2 (approximately 48.1 acres) and associated preliminary development plan for Prairie Trace.
- b. **Z-20-02:** Hold a public hearing on and consider a rezoning of approximately 31.9 acres from County PRB2 and PEC3 District to City of Gardner District R-3.
- c. **Z-20-03:** Hold a public hearing on and consider a rezoning of approximately 100.4 acres from County and PEC3 Districts to City of Gardner District C-3.
- d. **PP-20-01:** Consider a preliminary plat 356 single-family lots for Prairie Trace.

3. **FRONTIER COMMUNITY CREDIT UNION**

Located southwest of the intersection of Madison Street and Moonlight Road

- a. **PP-20-02:** Consider a preliminary plat for a 2 lot, 3.23 acre commercial subdivision.
- b. **FP-20-01:** Consider a final plat for a 2 lot, 3.23 acres commercial subdivision.
- c. **SP-20-01:** Consider a site plan for the Frontier Community Credit Union.



In compliance with the Americans with Disabilities Act, the City of Gardner will provide reasonable accommodations for all public meetings. Persons requiring accommodations in attending any of our public meetings should contact the City Clerk's office at 856-0945 a minimum of 48 hours prior to the meeting.

4. **COMPREHENSIVE PLAN AMENDMENT**

**CPA-20-01:** Hold a public hearing and consider adopting the *Gardner Destination Downtown Plan* and incorporating by reference the Plan into the *Gardner Comprehensive Plan*.

5. **ELECTION OF A VICE-CHAIR**

Nominate and elect a Vice-Chair of the Planning Commission for a term to expire July 2020.

**DISCUSSION ITEMS**

1. **SMALLER LOT SINGLE-FAMILY HOUSING IN GARDNER LAND DEVELOPMENT CODE**

**ADJOURNMENT**





## **PLANNING COMMISSION MEETING**

City of Gardner, Kansas  
Tuesday, January 28, 2020  
7 p.m.  
Gardner City Hall  
120 E. Main Street

### **CALL TO ORDER**

The meeting of the Gardner Planning Commission was called to order at 7:00 p.m. on Tuesday, January 28, 2020, by Chairman Scott Boden.

### **PLEDGE OF ALLEGIANCE**

Chairman Boden led the Pledge of Allegiance.

### **ROLL CALL**

Commissioners present:

Chairman Boden  
Commissioner Deaton  
Commissioner Ford  
Commissioner Hansen  
Commissioner Meder  
Commissioner McNeer

Commissioners Absent:

Commissioner Simmons-Lee

Staff members present:

Larry Powell, Director, Business & Economic Development  
Kelly Drake Woodward, Chief Planner  
Michelle Leininger, Principal Planner  
Robert Case, Planner  
Kristie Hatley, Planning Technician  
Ryan Denk, City Attorney

There were twelve members of the public in attendance.

### **CONSENT AGENDA**

#### **1. Approval of the minutes as written for the meeting on November 19, 2019.**

**Motion made by Commissioner Meder to approve the minutes with the recommendation by Commissioner Hansen to correct the spelling of the word “effective” under the Discussion Items on page 5.**

**Motion seconded by Commissioner Ford.**

**Motion passed 6-0.**

## **REGULAR AGENDA**

### **1. SYMPHONY FARMS AMENITIES AREA**

**FDP-19-04:** Consider a final development plan for an amenities area for Symphony Farms located at the northwest corner of Kill Creek Road and W 167<sup>th</sup> Street.

Ms. Michelle Leininger, Principal Planner, presented the final development plan for this project located at Kill Creek Road along the east and W 167<sup>th</sup> Street along the south. Currently under construction are the pool and restroom/pool equipment building. Future amenities will include a community garden, clubhouse and sports court. The buildings will reflect the architectural theme of the existing historic barn. Staff has found this plan consistent with the Comprehensive Plan and the site is capable of accommodating the proposed buildings and site design elements. Quality architecture and building design and materials are proposed, and the plan is compatible with both the context of the area and the approved preliminary development plan. Staff recommends approval of FDP-19-04 with conditions.

The applicant was present to answer questions.

## **COMMISSION DISCUSSION**

No discussed ensued.

**Motion made after review of application FDP-19-04, a final development plan for Symphony Farms Amenities Area on property located at the northwest corner of Kill Creek Road and W 167<sup>th</sup> Street (Tax Ids CP84410000 0001 and CP84410000 0002) and final development plan document dated August 16, 2019, and staff report dated January 28, 2020, the Planning Commission approves the application subject to the following conditions:**

- 1. The Administrative Plat shall be recorded prior to the issuance of a building permit.**
- 2. Provide No Parking (R8-3) signs: On the east side of Kill Creek, 100 ft north of 167<sup>th</sup> St and North of 166<sup>th</sup> St.**
- 3. Provide No Parking (R8-3) signs: On the west side of Kill Creek, south of 166<sup>th</sup> St and between the parking lot drives, with an additional “Here to Corner” (R7) sign.**

**Motion made by Meder and seconded by Deaton.**

**Motion passed 6-0.**

### **2. HILLTOP RIDGE**

Located at the southwest corner of Kill Creek Road and W 167<sup>th</sup> Street

- a. **Z-19-05:** Consider a rezoning from RUR (Rural, Agricultural uses and single family dwellings) District to R-1 (Single-Family Residential) District for Hilltop Ridge.
- b. **PP-19-06:** Consider a preliminary plat for Hilltop Ridge, a 71.15 acre, 150 lot single-family residential subdivision.

Mr. Robert Case, Planner, began the presentation with Z-19-05, the rezoning of 71 acres.

This request is compatible with the surrounding properties and appropriate for the character of the area which is used as farmland or single-family homes. It was also consistent with the City's Comprehensive Plan. When in the county, the property was zoned for agricultural and large lot residential which was an appropriate residential density when utility infrastructure was unavailable.

The area is urbanizing with the planned growth of Gardner and staff has found the property better suited for an R-1 designation. The removal of the property's current restriction of the minimum ten-acre lot size will result in increased density, traffic and stormwater in the area. However, the applicant is providing for the continuation of Madison Street that will provide another important road connection for existing subdivisions to the south and improve public safety. This single-family development will provide a positive gain to the City's economic growth and the development of new streets will provide interconnectivity between neighborhoods. The applicant's proposal offers a more efficient means of travel within the northwest area of the City. Staff recommends approval of the rezoning as it will not adversely affect the capacities of the utilities, infrastructure or public services in the vicinity. The application to rezone to R-1 single-family development supports the adopted policy of the Comprehensive Plan for the new residential growth areas. It also supports the extension of the existing trail system through its interconnection with the Kill Creek trail future location, which is consistent with the adopted Park System Master Plan.

Mr. Brett Cox, Renaissance Infrastructure, represented the applicant and was available to answer questions.

### **PUBLIC HEARING**

Mr. Tim Coach, 32580 W 171<sup>st</sup> St, stated he has three areas of concern. The first is the low water pressure they are experiencing but said he is aware the City was just doing a water study of the area. Secondly, he said that 172<sup>nd</sup> and Osage is already a drag strip and the proposed design with Osage becoming a through street past Madison to an area further down the hill is just a longer runway for the teenagers. Lastly, his most important concern is the existing trees that are almost 90 years old that he does not want to lose. These are on the south side of Madison and are of high importance to the area.

Ms. Traycie Haney, 32626 W 171<sup>st</sup> St, said her backyard is against those trees and she chose that lot because of the gorgeous view. Everyone in the neighborhood comments how much they like living there because of the beautiful view of trees. They need to be maintained and not torn out to be replaced with new trees.

Ms. Lisa Culbertson, 32500 W 171<sup>st</sup> St, stated she wants the trees to remain, too. Her front yard is 171<sup>st</sup> St with Jessica St teeing into her front windows. Her backyard will be Madison St so she wanted to know the proposed size of the planned easement between her backyard and Madison St. She also inquired if there would be a sidewalk on her side of the street so she could plan for fencing, if needed.

**Motion to close the Public Hearing made by McNeer and seconded by Meder.**

**Motion passed 6-0.**

### **COMMISSION DISCUSSION**

No discussed ensued.

**Motion made after review of case Z-19-05, a rezoning from RUR (Rural, Agricultural uses and single family dwellings) District to R-1 (Single-Family Residential) District for Hilltop Ridge, Tax Id 2F221422-1003, located on the south side of 167<sup>th</sup> Street approximately 200' west of Kill Creek Road, and a staff report dated January 28, 2020, the Planning Commission recommends the Governing Body approve the application.**

**Motion by Ford and seconded by McNeer**

**Motion passed 6-0.**

Mr. Case continued the presentation with the preliminary plat review. The preliminary plat proposed by the developers includes 150 single-family lots with all utilities located along both W 167<sup>th</sup> St and Osage St. Madison St will be extended along the southern boundary of the plat and a proposed trail extends along the creek to W 167<sup>th</sup> St that will eventually connect with the Kill Creek trail system. Staff has found the application in is in accordance with the Comprehensive Plan and compliant with the Land Development Code. The Comprehensive Plan calls for plans in the new residential growth areas to implement a sidewalk/trail infill program that includes pedestrian through-access connecting subdivisions. This plan connects with St. Johns Trace Subdivision to the south and with the Symphony Farms Subdivision to the north. The proposed phasing is clearly indicated in the application and demonstrates a logical and coordinated approach to development. Impacts identified by specific studies or technical reports, including a preliminary review of storm water, are mitigated with generally accepted and sound planning, engineering, and urban design solutions that reflect long-term solutions and sound fiscal investments. The application does not deter any existing or future development on adjacent property from meeting the goals and policies of the Comprehensive Plan nor impedes the construction of anticipated or planned future public infrastructure within the area. Staff recommends approval.

Mr. Brett Cox, Renaissance Infrastructure, represented the applicant and said they were in agreement with all of the conditions and were working on some minor details on the storm water study. He responded to the questions by the public by stating they planned to match grade on Madison St. as much as possible to save trees and minimize impact by the existing houses.

### **COMMISSION DISCUSSION:**

Commissioner Meder asked about the staff report note regarding the fire department request for a secondary access to the south after phase two of the development is completed. She felt it would help alleviate some of the traffic problems and asked if that should be added as a condition on this motion.

Mr. Larry Powell, Director of Business & Economic Development, replied to earlier questions before answering Commissioner Meder. He said the right-of-way for Madison St is 60' wide, and is directly adjacent to the property line with no right-of-way taken from St. John's Trace nor the backyards of any of the houses along the north side of St. John's. Between the property line and the street is a 15' grassy, treed area on the south side of what will be the new Madison roadway. There will be a stop sign where Osage intersects Madison from the north and the south. The through street is broken up to avoid the same long run as there is for the existing homes.

It was suggested by the public to add a flashing stop sign at that intersection as people may run the stop sign.

Mr. Powell continued to say if there is still a problem with the raceway then a possible solution is to add a stop sign somewhere along the route. The police department could look at that if that is an issue.

The public in attendance felt the stop sign at Osage and Madison should take care of the issue.

Mr. Powell said staff would look at the sidewalk requirement on both sides of Madison and if the south sidewalk was not necessary it could be removed from the plan and that would extend the grass and tree area. Staff will make note of that request.

Commissioner Meder asked if the location of the second access required by the fire department was known.

Mr. Powell responded that dependent upon construction of phase two, the applicant has the choice to either go up to the middle to where the Osage St connection is and go that way or they could go to the right along the west side and hit where Madison will come into existence and take Madison over. Either way is appropriate for providing the secondary access to the south.

It was decided not to add the condition to the motion regarding the secondary access but due to its importance, staff would make note that it would continue to be addressed.

**Motion made after review of case PP-19-06, a preliminary plat for Hilltop Ridge, Tax Id 2F221422-1003, located on the south side of 167<sup>th</sup> Street approximately 200' west of Kill Creek Road, and staff report dated January 28, 2020, the Planning Commission recommends the Governing Body approve the application subject to the following conditions:**

- 1. Provide a 10' wide trail along 167<sup>th</sup> Street and also extends down along the floodplain located on the northwest side of the development.**
- 2. Provide 60' of ½ right-of-way along 167<sup>th</sup> Street.**
- 3. Approval of a Stormwater Management Plan by the Public Works Department.**

**Motion made by McNeer and seconded by Ford.**

**Motion passed 6-0.**

### 3. TEXT AMENDMENT

- a. **TA-20-01:** Hold a public hearing and consider proposed text amendment TA-20-01 to the *Gardner Land Development Code* regarding the Public Utility Facility-Major land use.
- b. Initiate text amendments to various sections of Title 17 Land Development Code of the Gardner Municipal Code enabling regulations that are consistent with the context of rural areas of Gardner and that preserve rural character.

Ms. Kelly Drake Woodward, Chief Planner, began her presentation of text amendment TA-20-01 stating that specific standards for public utility and airport uses were adopted into the LDC in 2017. Utility uses were classified as either minor or major. Major public utility facilities include generating plants, electrical switching facilities and primary substations, water and wastewater treatment plants, and other similar facilities that provide the general public with services regionally. Currently, in the agriculture and residential zoning districts, minor public utilities are a conditional use subject to additional standards, and major public utilities are not permitted. In the commercial and industrial zoning districts, minor public utilities are a permitted use, and major public utilities are a conditional use, all subject to additional specific use standards. The proposal is to allow major public utilities in the additional zoning district of A – Agriculture as a conditional use subject to additional specific use standards.

This amendment was initiated by the Governing Body to consider allowing the major public utility facility use in additional districts for a more efficient process. Conditional uses, and their expansions, entail a public hearing and recommendation by the Planning Commission with final approval by the Governing Body. The use can be approved with additional conditions to address any other concerns arising from the context, thereby mitigating any potential community concerns while supporting essential public services in an efficient manner. There are ten review criteria for conditional uses which pertain to furthering the intent of the zoning district and adjacent districts, having a positive impact on the public realm, ensuring adequate drainage and public utilities, and being found to be compatible with the character of the area in design and function. All Public Utility Facility – Major uses are subject to additional specific use standards regulating setback from residential districts, height transitions, landscape buffers, and fencing.

As the community grows, there is a need to provide public utility services to outlying areas. Based on a comparison of the adopted utility plans and future land use maps, it may be advisable that some major public utility facilities be developed on sites planned for agriculture or residential zoning. Currently, new major public utility facilities would have to be built on land that is rezoned to a commercial or industrial district, even though the facilities would perhaps be located adjacent to agriculture or residential uses. Then, if the facility was ever decommissioned or redeveloped, the site would potentially be opened up to various commercial and industrial uses that may not be compatible with the adjacent land uses unless the property was rezoned before redevelopment. Of additional concern, the LDC provides that nonconforming uses (that were legally initiated prior to the adoption or amendment of this Code, but would not meet the terms of the current code) may continue to exist (or be grandfathered in), but may not be expanded beyond the existing area of the lot. The City's wastewater treatment plant is located on a site zoned Agriculture District near Celebration Park along W 159<sup>th</sup> St. Without the amendment, this facility would have to be rezoned to be expanded. Examples of existing and planned facilities that would be affected if this amendment was not implemented were presented, including a new electric substation and wastewater treatment plant planned south of I-35. Staff recommends approval of this text amendment.

Commissioner Ford asked what would be considered “major” facilities.

Ms. Woodward replied major facilities would provide services in a more regional way such as electrical facilities, primary substations, water and wastewater treatments plants and natural gas. She said it was somewhat subjective and allowed staff to look at what was being planned to determine if it should be processed as a major or minor utility. Most of the utilities in Gardner are locally operated but considered major because they provide access to a broader area.

### **PUBLIC HEARING**

No comments from the audience.

**Motion made by Meder to close the Public Hearing and seconded by Hansen.**

**Motion passed 6-0.**

### **COMMISSION DISCUSSION:**

Chairman Boden asked what types of transitions or buffers would be built for the utility facilities in the agricultural district and if they would be installed when the facility was constructed or at a later time and how would you provide for a buffer if the surrounding property is undeveloped.

Ms. Leininger explained that staff would look at the city’s Comprehensive Plan to determine future anticipated uses. Then, because it was a conditional use permit, everything would be taken into consideration, including the need for more or less landscape or setback.

Ms. Woodward responded the transitions would be landscape buffers and would be installed when the facility was developed. The specific use standards for the public utility facility-major use provide that they are subject to level 4 buffer requirements just like an industrial facility.

The commissioners discussed the conditional permit process.

**Motion made to recommend that the Governing Body approve text amendment TA-20-01 to the *Gardner Land Development Code*, revising the use provisions for Communications and Utilities to permit Public Utility Facility – Major as a Conditional Use subject to specific use standards in the additional zoning district of A (Agriculture) District as presented in the January 28, 2020 staff report.**

**Motion made by Hansen and seconded by Meder.**

**Motion passed 6-0.**

Ms. Woodward continued with her presentation on item 3b to initiate potential text amendments that focus on Gardner’s new growth areas and enabling regulations that are

consistent with the context of rural areas and preserve rural character. Since the LDC was adopted, some County properties have been annexed into the City. The City has also adopted subarea plans for areas south of I-35 that include a goal to maintain rural character in existing areas until the time that municipal services and market factors allow urban densities to develop. The plans call for residential development patterns such as conservation or cluster development to preserve larger areas for conservation, open space, habitat, regional water management, and agriculture use. The LDC was designed to facilitate a more urban context. Because the city is expanding into rural areas, staff has found the need to look at the regulations to see how to maintain that rural character until the facilities are available there to serve development. If that is allowed to happen organically, it may become difficult later to try to develop a greater density.

Topics staff would like to address to retain rural character and context:

- **Rezoning for annexed land –**
  - LDC provides that all land that is annexed shall be classified as the corresponding city zoning district most similar to existing county zoning district. However, the City Attorney has said that this automatic reclassification is not supported by case law, and a formal process must be undertaken to achieve this reclassification.
  - It may be beneficial to waive the rezoning fee for annexed parcels provided that the reclassification is to the corresponding city zoning district most similar to existing county zoning district, or to a district that is consistent with the future land use map.
- **Process for larger land divisions –**
  - Lot splits for larger parcels, including a width to depth ratio.
  - Currently, the creation of lots less than five acres triggers the plat process, and the LDC does not offer standards for the division of lots larger than this until development occurs. However, it is important to have standards for these larger lot splits so that reasonable development and access will be feasible in the future.
- **Conservation subdivisions** - Planned District/incentives provisions to facilitate subdivisions that preserve large, connected natural areas and offer more compact housing patterns where appropriate utilities can be provided.
- **Excise tax** – the possible deferral of excise tax with plats where development is not yet imminent.
- **Rural road network** - Subdivision standards impacting road networks, access streets and connectivity.
- **Rural road design** - street type standards that address sidewalks and whether to allow new gravel roads.
- **Private road requirements** – provision for private roads with public access.
- **Rural open space** types and requirements.
- **Rural frontage type** - A frontage type that addresses rural character. The most likely frontage type for a rural home would be the suburban yard. Although these could be reasonably applied, including access width limits a max of 24' at the ROW, required trees based on frontage length, and hardscape limit of 40%, staff may need to address screening of other typical front yard activities in rural areas.
- **Rural building type** - Potential building types or building type modifications for rural context. This would most likely entail relaxing some design and performance standards that are more appropriate in areas with a closer relationship to the public realm.
- **Fencing and accessory buildings** - Accommodation of rural livestock fencing and customary accessory buildings in rural development patterns.
- **Outdoor storage and vehicle parking** – different screening and location options.



- **Rural uses** - review of use standards with the context of this area in mind.
- **Building materials and design standards** – building and design that is appropriate for use within a rural context.
- **Buffer and screening** requirements.
- **Stormwater** requirements – opportunities for more regional solutions?
- **Property maintenance** pertaining to vegetation.

Staff requests that the Planning Commission initiate amendments to all applicable sections of the LDC to address these issues and goals.

### **COMMISSION DISCUSSION**

No discussed ensued.

**Motion made to initiate text amendments to multiple sections of the Title 17 Land Development Code of the Gardner Municipal Code to address issues as presented in the January 28, 2020 staff report, or as subsequently presented, enabling regulations that are consistent with the context of rural areas of Gardner and that preserve rural character.**

**Motion made by Deaton and seconded by Ford.**

**Motion passed 6-0.**

#### **4. ELECTION OF A VICE-CHAIR**

Nominate and elect a Vice-Chair of the Planning Commission for a term to expire July 2020.

An election was held but the bylaws state the election of an officer must have a minimum of 4 votes to pass. Two candidates were nominated for Vice-Chair however neither received the number of votes needed. Mr. Powell suggested the election of the Vice-Chair tabled until the next meeting to bring it before all seven members of the Planning Commission.

**Motion to table the election of Vice-Chair to the February meeting made by Ford and seconded by Deaton.**

**Motion passed 6-0.**

### **DISCUSSION ITEMS**

#### **1. GARDNER DESTINATION DOWNTOWN PLAN**

Ms. Woodward gave a brief overview about the study area and the history of the project that began with the *Gardner Main Street Corridor Plan*. The Plan focused on the big picture and crafted a vision that integrated transportation, land use and the environment while collaborating with the community. She presented a short video featuring the Corridor Plan.

The *Gardner Destination Downtown Plan* is the second level of planning and focuses on project development and implementation activities for a smaller, 8-block study area of downtown. She described the steps taken by the steering and implementation committees, community stakeholders, and staff to devise this plan including a Kansas City metro bus tour of similar projects, Walk & Roll tour of Downtown, public events, online surveys, public meetings and meetings with property owners. Mr. Terry Berkbuegler of Confluence, the consultant for the city, presented the latest draft of the *Gardner Destination Downtown Plan*. A question was asked about parking for City Hall and was answered by Mr. Berkbuegler that all existing parking was relocated within the plan along with additional parking spaces overall. The plan will be presented for adoption at the February meeting.

## **2. ANNUAL REVIEW OF THE COMPREHENSIVE PLAN**

Ms. Woodward included within the staff report a history of the Comprehensive Plan and subsequent amendments since its adoption in 2014, including the adoption of additional plans by reference. The *Gardner Destination Downtown Plan* would be the next amendment, but several items were initiated as possible amendments in 2018 that had not yet been implemented. Additional items that could be considered for amendment were identified in a joint City Council/Planning Commission meeting in December 2018. Staff recommended additional action items for 2020 plan amendments. She asked the Planning Commission to initiate these items for anticipated Comprehensive Plan amendments.

**The Planning Commission receives the annual review of the Comprehensive Plan and initiates amendments as identified in the January 28, 2020 staff report for implementation as anticipated amendments to the Comprehensive Plan in 2020, specifically those items identified during the 2018 joint Planning Commission/Governing Body meeting and additional recommended items for 2020.**

**Motion made by Meder and seconded by Ford.**

**Motion passed 6-0.**

## **3. 2019 ANNUAL PLANNING COMMISSION ACTIVITY RECAP**

Ms. Woodward reviewed each of the projects brought before the Planning Commission for the past few years to update them on the progress of the City.

## **ADJOURNMENT**

**Motion to adjourn made by Hansen and seconded by Meder.**

**Motion passed 6-0.**

Meeting adjourned at 9:12 pm



## Planning Commission

January 28, 2020

<u>Name</u>	<u>Address</u>
KEN KELTNER	GARDNER
BRETT COX - Renaissance Twp.	5015 NW Canal, Riverside Mo
JAMES HUMBERT	<del>100</del> 16670 HILL CREEK
Tim & Tanya Coack	32980 W. 171st Gardner
Traycie Haney	32626 W. 171st St.
Randy Thyl-Skuff	Gardner
Lisa Culbertson	32500 W. 171st

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**PROJECT NUMBER / TITLE: FDP-20-01 New Life Community Church**

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**PROCESS INFORMATION**

**Type of Request:** Final Development Plan  
**Date Received:** January 10, 2020

**APPLICATION INFORMATION**

**Applicant:** Phelps Engineering, Inc. – Doug Ubben, Jr.  
**Owner:** New Life Community Church, Inc. – Rick George  
**Parcel ID:** CF221436-2019 and CF221436-2018  
**Location:** North side of 188<sup>th</sup> Street approximately 770' East of Gardner Road

**REQUESTED ACTION**

The applicant is requesting approval of a final development plan for a church.

**EXISTING ZONING AND LAND USE**

The property is currently zoned ACP-2 (Activity Center Planned General Business) District and is undeveloped.

**SURROUNDING ZONING AND LAND USE**

<b><u>Zoning</u></b>	<b><u>Use(s)</u></b>
<b>North of subject property</b>	
R-1 (Single Family Residential) District	Single-family homes and large retention area with Electric Transmission Lines
<b>East of subject property</b>	
C-3 (Heavy Commercial) District	Vacant Property
<b>South of subject property</b>	
C-2 (General Business) District	Vacant Property
<b>West of subject property</b>	
ACP-2 (Activity Center Planned General Business) District	Planned Commercial Development (Plaza South)

**EXISTING CONDITIONS**

The lots are currently undeveloped and have been used for agricultural purposes. Existing utility infrastructure including sanitary sewer and electric are located adjacent to the parcel. Along the entire northern property line is an approximately 270' deep tract used for stormwater detention basins for the Aspen Creek subdivision and overhead power lines. Currently 188<sup>th</sup> Street east of Gardner Road is not paved beyond the portion that accesses Groundhouse South at the southeast corner of the S. Gardner Road and W. 188<sup>th</sup> Street intersection, but there is an existing

gravel access drive to an interior property abutting I-35. There appears to be no floodplain on the property, although there are some areas designated for potential flooding to the east.



### **BACKGROUND / HISTORY**

This property is part of the Plaza South preliminary development plan and preliminary plat, approved in March, 2019. The final development plan is consistent with the preliminary development plan.

The rezoning and associated preliminary development plan (PDP-19-02, and PP-09-03) were recommended for approval by the Planning Commission at their March 26, 2019 meeting. The Governing Body considered the rezoning and preliminary development plan applications at their April 15, 2019 meeting, and approved Ordinance 2612 rezoning the property from C-2 (General Business) District to ACP-2 (Activity Center Planned General Business) District and approving the associated preliminary development plan subject to the following conditions:

1. The trail along W. 188<sup>th</sup> Street shall be constructed of concrete.
2. The applicant shall provide for a common area maintenance agreement or property maintenance agreement for all common areas at time of the first final development plan/final plat applications, including the infrastructure within all access and sidewalk easements.
3. All easements shall be approved prior to approval of any final development plan.
4. The stormwater plan and traffic study shall be approved prior to approval of any final development plan.
5. Lots 5, 6, 7, 8 and 9 – To better meet the intent of the required building frontage within this development context, shall include an acceptable vertical element along at least 35% of the front building line, either via a building, plantings, or an ornamental wall or fence complimenting the design of the buildings.

These conditions have all been met.

The Preliminary plat (PP-19-03) was approved by the Planning Commission on March 26, 2019, subject to the following condition:

1. Approval of the Traffic Impact Study and Stormwater Management Plan by City of Gardner Public Works Department.

This condition has been met.

The Final Plat (FP-19-04) was approved by the Planning Commission on October 22, 2019, with the following conditions:

1. Approval of the final Traffic Impact Study and Stormwater Management Plan;
2. Prior to the Mayor signing an approved recordable plat, the applicable excise tax shall be paid to the City;
3. The construction plans for any utilities, infrastructure, or public facilities shall meet all technical specifications and public improvement plans shall be submitted and approved prior to the release of the plat for recording.

This condition has been met.

The Governing Body accepted the dedication of right-of-way and easements on November 18, 2019.

### **CONSISTENCY WITH COMPREHENSIVE PLAN**

The *City of Gardner Comprehensive Plan* and the *I-35 & Gardner Road Interchange Subarea Plan* identifies the properties for Regional Commercial future land uses. This use group provides for goods and services that may attract users from the greater metropolitan area. These uses are generally on larger parcels and are best located near interstates and major arterial roads. This site is within a half mile of the I-35 and Gardner Road interchange and along S. Gardner Road which is an arterial street. 188<sup>th</sup> Street is a proposed collector street to be extended to the east with this plat. The proposed use, Assembly – large, is for a regional church that is consistent with the Comprehensive Plan.

The proposed development is also consistent with the Comprehensive Plan as follows:

- Commercial area objective to promote commercial growth at the I-35 interchanges to provide neighborhood and regional commercial areas with a diverse range of retail and commercial uses and services.
- Open Space Recreation & Environmental Features objective to increase public bicycle and pedestrian access to community facilities through connections to the local trail system.
- Commercial Areas Plan for Regional Commercial to site buildings to create internal “streets” that are attractive and comfortable for pedestrians and integrate public amenities such as trails.

### **DEVIATION REQUEST**

#### **Chapter 17.07 BUILDING STANDARDS**

##### **Section 17.07.050 Frontage Design: Terrace frontage type**

###### **Standard:**

Access width limit = 20% **OR** up to 20'

**Proposed:** 31' access for east drive onto 188<sup>th</sup> Street.

**Staff Comment:** The access width limits for Terrace Frontage would possibly cause congestion at this access point with the number of vehicles that will be arriving and leaving this site at the same time. The large flared entrance also allows for larger trucks to be able to make deliveries without driving over the curb. Staff is supportive of this deviation.



## **STAFF ANALYSIS - FINAL DEVELOPMENT PLAN**

This section highlights contents of the application which may merit particular consideration in regard to zoning intent and standards. If a standard is not presented, it has been met. Standards not met will be addressed in conditions of approval at the end of this report. A full analysis to all applicable regulations is available on request.

This application is being reviewed per the Planned Development process of Section 17.03.040 of the LDC, and specifically Section 17.06.040 Activity Center Planned District. The uses for this district are based on the typically applicable base zoning district (C-2) associated with the Comprehensive Plan Future Land Use category for the site (Regional Commercial).

### **17.03.040 (F) Review Criteria:**

- In general, any final development plan in compliance with all requirements of this Code shall be approved.

**Staff Comment:** *The final development plan is generally in compliance to the Code with the exception of the approved deviations from the preliminary development plan and recommended conditions of approval as follows:*

- *Substitute deciduous trees for the northern-most trees adjacent to the deferred parking, and space along the perimeter of the parking lot.*
  - *Provide a direct, continuous sidewalk from the Church to the deferred parking lot.*
2. In making a determination of compliance, or for final development plans accompanying any discretionary review or administrative relief, the review body shall consider whether:
- a. The site is capable of accommodating the buildings, proposed use, access and other site design elements required by the Code and will not negatively impact the function and design of rights-of-way or adjacent property.

**Staff Comment:** *The final development plan is generally consistent with the preliminary development plan. Items such as building standards and landscaping requirements are reviewed under the current Land Development Code (LDC). This plan has met all of the applicable standards with the exception of the landscape plans. The applicant will need to correct the calculations as provided on the landscape plans and will be a condition of approval for the final development plans.*

- b. The design and arrangement of buildings and open spaces is consistent with good planning, landscape design and site engineering principles and practices.

**Staff Comment:** *The design and arrangement of the building and open spaces is consistent with good design, principles and practices. The site includes features and amenities that establish a built relationship with adjacent sites and the public realm.*

- c. The architecture and building design uses quality materials and the style is appropriate for the context considering the proportion, massing, and scale of different elements of the building.

**Staff Comment:** *The building design and size is appropriate with the size of the lot and its close proximity to the interstate. The elements are proportional to the building and create a unique structure within this side of the community. Materials include decorative siding and a mix of metal and natural materials.*

- d. The overall design is compatible to the context considering the location and relationships of other buildings, open spaces, natural features or site design elements.

**Staff Comment:** *The overall design is compatible to the context of the surrounding properties as discussed above.*

- e. Whether any additional site-specific conditions are necessary to meet the intent and design objectives of any of the applicable development standards.

**Staff Comment:** *A few trees will need to be changed along the north parking lot, other than this condition, no additional site specific conditions are necessary.*

- The application meets the criteria for all other reviews needed to build the project as proposed.

**Staff Comment:** *The applicant shall be required to submit and receive approval of public improvement plans for the extension of a public water main in order to add an additional fire hydrant onsite. An existing utility easement can be utilized for this extension onsite. This will be a condition of approval.*

- The recommendations of professional staff.

**Staff Comment:** *Staff recommends approval of the final development plan for New Life Community Church to be located on the north side of 188<sup>th</sup> Street approximately 770' east of Gardner Road with the conditions outlined in the recommendation section of this staff report.*

- The final development plan is in substantial compliance with the approved preliminary development plan.

**Staff Comment:** *The final development plan is in substantial compliance with the approved preliminary development plan.*

#### **DEVIATIONS (Previously Approved)**

Section 17.06.010 of the Land Development Code states "Departures from the standards associated with all zoning districts may be considered through planned zoning, and approved provided they are based upon a comprehensive and well-integrated development plan for the area. These departures shall not be justified for simply wanting flexibility or deviations from standards based upon a site plan." **The deviations that were request by the applicant and approved at the March 26, 2020 Planning Commission meeting as pertaining to Lot 9 are listed below:**

#### **Chapter 17.07 BUILDING STANDARDS**

##### **Section 17.07.050 Frontage Design: Lots 5,6,7,8 and 9: Terrace frontage type**

##### **Standard:**

Required building frontage = **70%** (a minimum of 70% of the lot frontage that shall be occupied by the principal structure at the front building line – appropriate for buildings which have a 10' – 25' setback from the street). An exception may be granted for up to 50% of this requirement through the administrative adjustment process, provided:

- a. The site provides active pedestrian space comparable to the Open and Civic Space System; and
- b. A vertical element such as a low hedge, ornamental wall, or fence complementing the design and materials of the building is substituted as an extension of the building line.

**Proposed:** Lots 5-9 - 0% of the frontage is occupied by the principal structures within the "appropriate" 10' – 25' setback range.



**Applicant Response:** A deviation is requested for the required building frontage of 70%. Additional landscaping, berming or other feature would be provided on these lots as an amenity. This will be detailed further on the final development plans.

**Staff Comment:** Staff is supportive of this deviation. The applicant has provided sufficient plantings along with other features that meet the 35% vertical elements along the front building line.

### **Chapter 17.07 BUILDING STANDARDS**

#### **Section 17.07.050 Frontage Design: Lots 5,6,7,8, and 9 Terrace frontage type**

##### **Standard:**

Direct sidewalk connections at least 8' wide to the streetscape shall occur at least every 50'.

**Proposed:** The applicant has provided one required 8' wide sidewalk connecting the public sidewalk to the building entrance for each lot, and has also provided 5' wide "public" sidewalks along every private street.

**Applicant Response:** A deviation for direct sidewalk connections every 50 feet. Having a sidewalk every 50' would be not needed due to the large lot size and single user of the facility, and if sidewalks were placed every fifty feet they would not be used and be a maintenance and unnecessary expense to the owner.

**Staff Comment:** This requirement is meant to support walkable development patterns. As designed, each lot utilizing the Terrace Frontage Type has 2-3 pedestrian connections to the "public" sidewalk along the streets. Because of the configuration of the larger individual lots and internal vehicular circulation patterns, additional sidewalk connections would be ineffective and possibly unsafe. Staff recommends approving this deviation.

### **Chapter 17.07 SUBDIVISION STANDARDS**

#### **Section 17.07.040 Specific Building Type Standards: Lot 9, Prominent Civic Building Type**

##### **Standard:**

##### **Lot Size and Area**

2.5 – 5 acres; larger lot sizes permitted only as part of a campus plan through the planned development process that considers internal access and circulation with surrounding areas.

**Proposed:** This lot is 11.65 acres, far exceeding the range allowed.

**Applicant Response:** This request is due to the following factors:

1. Size of proposed church including 1092-seating sanctuary, Sunday school classrooms, church offices, Christian school, and basketball court that will also serve as a community use space.
2. Amount of parking required for church and community use basketball court.
3. Land dedicated for above ground BMPs and detention areas.
4. Open ground for use as soccer field and vacation Bible school outside activities, etc.
5. Open space on south side of property for community open space.

Staff Comment: Staff recommends approval of this deviation as the applicant is using the planned development process, and the plan is designed around internal access and circulation patterns that improve vehicular, pedestrian, and bicycle mobility and connect appropriately with surrounding areas.

## **Chapter 17.07 BUILDING STANDARDS**

### **Section 17.07.040 Specific Building Type Standards: Lot 9, Prominent Civic Building Type Standard:**

Building Placement – Front setback = 20' – 50'

**Proposed:** The building is placed at over 287' setback.

Applicant Response: There are several factors and constraints that are driving the need for the requested 287' front yard setback. The front of the church needs to face south to have the potential to be seen from Interstate I-35 to the south. Although a 20'-50' front setback would seem to be advantageous for better visibility to I-35, there are other constraining factors that necessitate the requested 287' building setback. These constraining design factors include the shape of the site, the orientation and configuration of the building footprint (due to economics and function), and location of the majority of parking spaces that need to have close proximity to the main entrance of the church. Since the lot is rectangular-shaped with the long leg running north to south, there is not enough room to have substantial parking on the east or west side of the building based on the configuration of the building footprint and width of the site.

We are proposing a community use site amenity area on the south side of the property along the 188<sup>th</sup> Street ROW to provide a use to meet the intent of a close Prominent Civic building type setback. This area will have park benches, picnic tables with shade structures, butterfly bushes, and limestone fence post along the walking trail.

**Staff Comment:** Staff encouraged the applicant to re-orient this prominent civic use nearer to the street as intended. Applicant response is included above. As an alternative, staff asked the applicant to include elements better addressing the streetscape, such as civic open space, public art, benches along the trail, a decorative street wall, etc. As a result, the applicant provided a winding trail within a 60' easement that includes park benches with shelter, picnic tables with shelters, and row of butterfly bushes. Other amenities will be addressed at time of final development plan, as they are not typically shown on a preliminary development plan. Staff recommends approval of this deviation based on the proposed amenities that establish a built relationship with the public realm.

## **STAFF ANALYSIS – INFRASTRUCTURE / OTHER**

### **UTILITIES**

Electric will be extended onto the site from 188<sup>th</sup> Street and will be looped back into the Plaza South development through a private street. The sanitary sewer is connected from the existing sewer main located along the north side of 188<sup>th</sup> Street. Water lines are connecting both from an existing main off of 188<sup>th</sup> Street and will be looped around the building. Stormwater facilities for the church are located along the east side of the site. Utilities are adequately provided for with this project. Public improvement plans will need to be approved before building permits are issued. *This is a condition of approval.*

## **ROADWAY NETWORK AND VEHICULAR ACCESS**

Access to the site is by way of both a private access road and 188<sup>th</sup> Street. Currently there are no paved access to this development. The Fire Department is requiring that before building permits can be issued, paved access to this site must be established to the site. *This will be a condition of approval.*

## **SIDEWALKS**

A 10' wide trail will be provided along the north side of 188<sup>th</sup> Street. 5' sidewalks are provided from the parking lots and extending around the building. A connecting 5' sidewalk will be provided to the commercial development to the west of this site.

## **FIRE**

Private fire hydrants will be placed around the building.

## **ATTACHMENTS**

- I. Final development plan documents
- II. Application

## **RECOMMENDATION**

Staff recommends approval of final development plan FDP-20-01 for New Life Community Church located on the north side of 188<sup>th</sup> Street approximately 770' east of Gardner Road subject to the following conditions:

1. The construction plans for any utilities, infrastructure, or public facilities shall meet all technical specifications and public improvement plans shall be submitted and approved prior to issuance of a building permit;
2. The final plat FP-19-04 shall be recorded with the Johnson County Records and Tax Administration prior to the issuance of a building permit;
3. Paved access meeting Fire District and City requirements shall be provided to the site prior to the issuance of building permits;
4. Approval of the final Traffic Impact Study and Stormwater Management Plan;
5. Substitute deciduous trees for the northern-most trees adjacent to the deferred parking, and space along the perimeter of the parking lot.
6. Provide a direct, continuous sidewalk/crosswalk from the Church to the deferred parking lot.

## **Recommended Motion:**

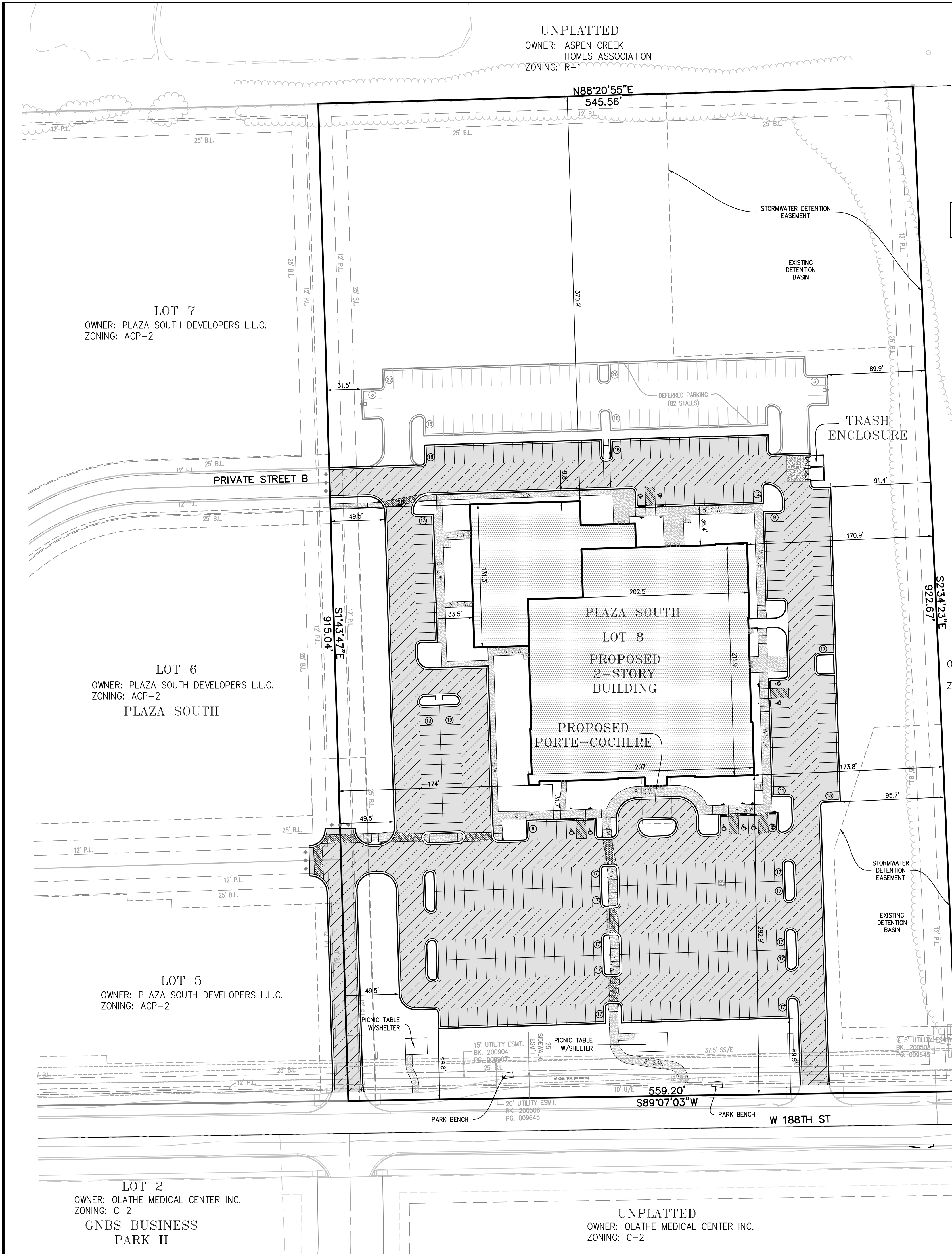
After review of Application FDP-20-01, a final development plan for New Life Community Church (Tax IDs CF221436-2019 and CF221436-2018), and final development plans dated February 14, 2020, and staff report dated February 25, 2020, the Planning Commission approves the application provided the following conditions are met:

1. The construction plans for any utilities, infrastructure, or public facilities shall meet all technical specifications and public improvement plans shall be submitted and approved prior to issuance of a building permit;
2. The final plat FP-19-04 shall be recorded with the Johnson County Records and Tax Administration prior to the issuance of a building permit;
3. Paved access meeting Fire District and City requirements shall be provided to the site prior to the issuance of building permits;
4. Approval of the final Traffic Impact Study and Stormwater Management Plan;

5. Substitute deciduous trees for the northern-most trees adjacent to the deferred parking, and space along the perimeter of the parking lot.
6. Provide a direct, continuous sidewalk/crosswalk from the Church to the deferred parking lot.



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UNPLATTED  
OWNER: ASPEN CREEK  
HOMES ASSOCIATION  
ZONING: R-1

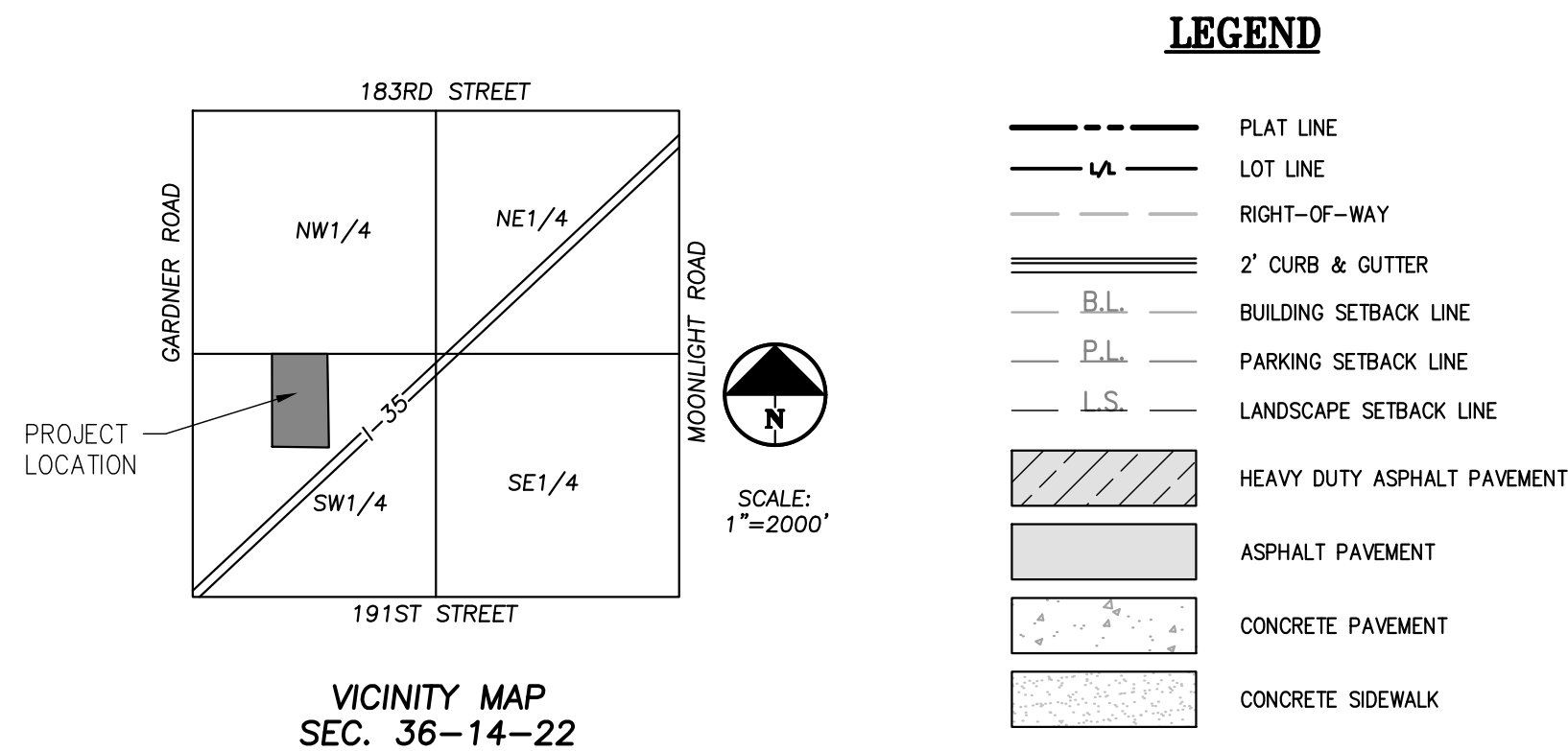
ZONING	
Zoning	ACP-2

LOT AREA DATA		
LOT 8	507472 SF	11.650 Ac.
LOTS TOTAL	507472 SF	11.650 Ac.

SITE DATA								
Lot 8 (Church) Total	Building Type	# of Stories	Total Building Area	Building Coverage	Lot Area	Building Coverage	Max. Code Coverage	Frontage Type
	Prominent Civic	2	71058 S.F.	53057 S.F.	507472 S.F.	10.5%	50.0%	Terrace
			71058 S.F.	FAR=0.14				

PARKING DATA									
LOT 8	Church (1,092 Seats)	Building 51900 S.F.	Ratio Required (1 per 4 Seats)	Vehicle Parking Required 273	Vehicle Parking Provided* 397	ADA Parking Required** 9	ADA Parking Provided 10	Bike Parking Required 14	Bike Parking Provided 15

\*Provided Parking Counts Includes "Deferred Spaces"  
\*\* Per 2010 ADA Standards for Accessible Design



LEGAL DESCRIPTION:  
ALL OF LOT 8, PLAZA SOUTH, A PLATED SUBDIVISION OF LAND  
IN THE CITY OF OVERLAND PARK, JOHNSON COUNTY, KANSAS.

- BENCHMARK:** VERTICAL DATUM = NAVD88 BASED ON JOHNSON COUNTY VERTICAL DATUM JOHNSON COUNTY BENCHMARK NETWORK BERNSTEIN ALUMINUM DISK STAMPED #1106 IN EAST END OF THE NORTH HEADWALL OF RCB ON NORTH SIDE OF 191ST STREET. ELEVATION = 1025.79
- A. FOUND "L" CUT IN CENTER OF FRONT FACE OF CURB INLET ON EAST SIDE OF GARDNER ROAD AT MIDDLE OF PHILLIPS 66.  
ELEVATION = 1044.12
- B. SET "L" CUT ON NORTHWEST CORNER OF CONCRETE PAD FOR ELECTRIC TRANSFORMER PAD AT NORTHEAST CORNER OF GROUNDHOUSE COFFEE.  
ELEVATION = 1046.77
- C. SET RAILROAD SPIKE WEST SIDE OF 5TH POWER POLE EAST OF GARDNER ROAD MIDDLE OF SOUTH PROPERTY.  
ELEVATION = 1050.08
- D. SET RAILROAD SPIKE WEST SIDE OF 8TH POWER POLE EAST OF GARDNER ROAD SOUTHEAST CORNER OF PROPERTY.  
ELEVATION = 1041.94

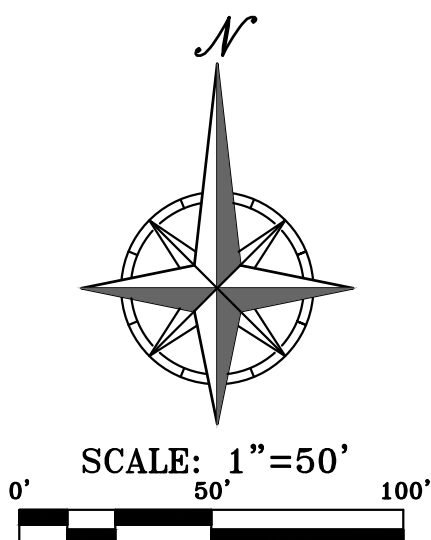
**FLOOD NOTE:**  
THIS PROPERTY LIES WITHIN ZONE X, DEFINED AS AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN, AS SHOWN ON THE FLOOD INSURANCE RATE MAP PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY FOR THE CITY OF GARDNER, COMMUNITY NO. 200164, JOHNSON COUNTY, KANSAS, MAP NO. 20091C0120G, AND DATED AUGUST 3, 2009.

- DEVIATION NOTES:**
- DEVIATION #1: INCREASE STANDARD LOT SIZE OF 2.5 TO 5 ACRES FROM THE DEVELOPMENT STANDARDS RANGE TO 11.65 ACRES.
- DEVIATION #2: INCREASE BUILDING SETBACK BETWEEN 20' AND 50' FOR PROMINENT CIVIC TYPE. INCREASE THE FRONT YARD SETBACK TO 293'±.
- DEVIATION #3: DEVIATION OF THE BUILDING FRONTAGE OF 70%. LOT 9 INCLUDES AMENITIES, TRAILS AND LANDSCAPING ALONG 188TH STREET.
- DEVIATION #4: DEVIATION FROM DIRECT SIDEWALK CONNECTIONS EVERY 50 FEET. SEE SITE PLAN FOR PROPOSED SIDEWALK CONNECTIONS

**ARCHITECT:**  
ROSE DESIGN BUILD, INC.  
P.O. BOX 100  
OLATHE, KS 66051  
913-782-0777 OFFICE  
913-782-0998 FAX  
CONTACT: CHRIS R. BELL, ALA  
EMAIL: chrisb@buildwithrose.com

**CIVIL ENGINEER:**  
PHELPS ENGINEERING, INC.  
1270 N. WINCHESTER  
OLATHE, KS 66061  
913-393-1155 OFFICE  
913-393-1166 FAX  
CONTACT: JUDD CLAUSSEN, P.E.  
EMAIL: jclausen@phelpsengineering.com

**OWNER/DEVELOPER:**  
NEW LIFE COMMUNITY CHURCH  
17935 MOONLIGHT ROAD  
GARDNER, KA 66030  
(913) 856-5683



PHELPS ENGINEERING, INC.  
1270 N. Winchester  
Olathe, Kansas 66061  
(913) 393-1155  
Fax (913) 393-1166  
www.phelpsengineering.com

PLANNING  
ENGINEERING  
IMPLEMENTATION



SITE PLAN - OVERALL  
NEW LIFE COMMUNITY CHURCH  
GARDNER, KANSAS  
FINAL DEVELOPMENT PLANS

PROJECT NO.	181177	No.	Date	By	App.	Revisions:	
						REVISED PER CITY COMMENTS	REVISED PER CITY COMMENTS
DATE: 1/10/20	DRAWN: JMO	1	2-3-20	JMO	ALN		
CHECKED: DEU	APPROVED:	2	2-11-20	JMO	ALN		
CERTIFICATE OF AUTHORIZATION							
LAND SURVEYING - LS-82							
ENGINEERING - E-361							
CERTIFICATE OF AUTHORIZATION							
LAND SURVEYING - 2007001/02							
ENGINEERING - 2007002/03							

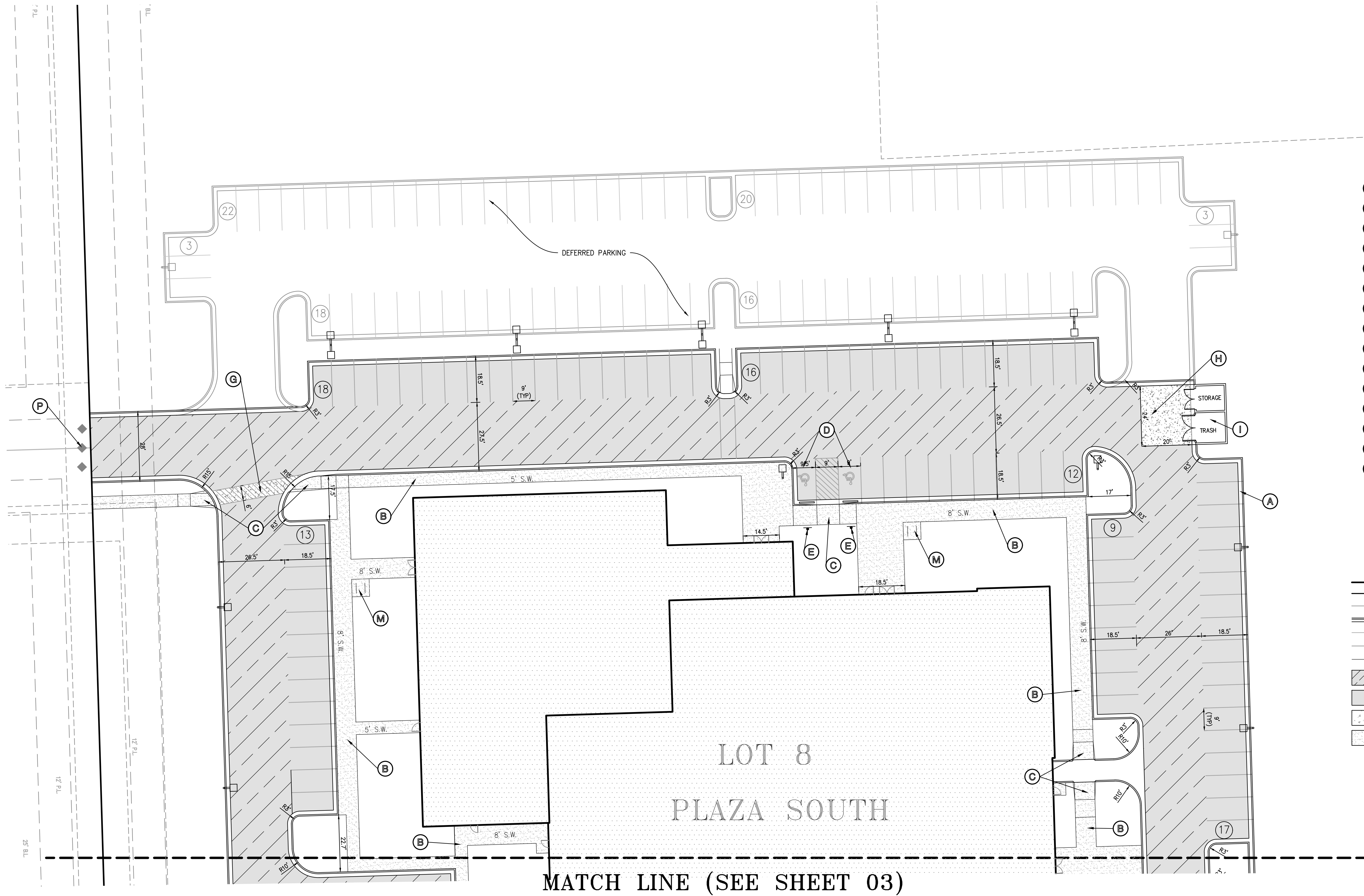
SHEET

01

OF 10



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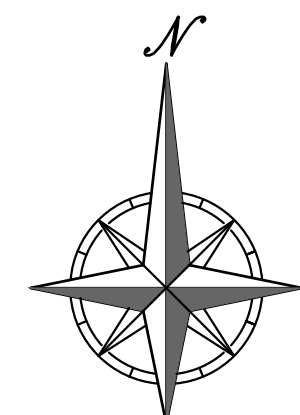


**SITE KEY NOTES:**

- (A) CONSTRUCT 2' CURB & GUTTER (TYPICAL).
- (B) CONSTRUCT PRIVATE CONCRETE SIDEWALK (TYPICAL).
- (C) CONSTRUCT PRIVATE SIDEWALK RAMP WITH LAYDOWN CURB & GUTTER.
- (D) CONSTRUCT ACCESSIBLE PARKING SPACE. INSTALL PAVEMENT MARKINGS, LAYDOWN CURB & BUMPERS. SEE ACCESSIBLE PARKING SPACE DETAIL.
- (E) INSTALL HANDICAPPED PARKING SIGN.
- (F) INSTALL VAN ACCESSIBLE PARKING SIGN.
- (G) CONSTRUCT SCORED CONCRETE PEDESTRIAN CROSSWALK.
- (H) INSTALL CONCRETE PAVEMENT.
- (I) INSTALL TRASH ENCLOSURE (REFER TO ARCH. PLANS).
- (J) INSTALL MONUMENT SIGN.
- (K) INSTALL PICNIC AREA.
- (L) CONSTRUCT CONCRETE PAD & INSTALL BENCH.
- (M) INSTALL BIKE RACK (5-STALLS).
- (O) INSTALL POLE SIGN.
- (P) INSTALL END OF ROAD MARKERS.

**LEGEND**

- PLAT LINE
- - - LOT LINE
- - - RIGHT-OF-WAY
- == 2' CURB & GUTTER
- B.L. - BUILDING SETBACK LINE
- P.L. - PARKING SETBACK LINE
- L.S. - LANDSCAPE SETBACK LINE
- [Hatched Box] HEAVY DUTY ASPHALT PAVEMENT
- [Solid Grey Box] ASPHALT PAVEMENT
- [Dotted Box] CONCRETE PAVEMENT
- [Stippled Box] CONCRETE SIDEWALK



SCALE: 1"=20'

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Olathe, Kansas 66061  
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Fax (913) 393-1166  
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PLANNING  
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IMPLEMENTATION



**SITE DIMENSION PLAN**  
**NEW LIFE COMMUNITY CHURCH**  
**GARDNER, KANSAS**  
**FINAL DEVELOPMENT PLANS**

PROJECT NO.	181177	No.	Date	By	App.
DATE: 1/10/20	DRAWN: JMO	1	2-3-20	JMO	ALN
CHECKED: DEU	APPROVED:	2	2-11-20	JMO	ALN
CERTIFICATE OF AUTHORIZATION					
LAND SURVEYING - LS-82					
ENGINEERING - E-391					
CERTIFICATE OF AUTHORIZATION					
LAND SURVEYING - 2007001/28					
ENGINEERING - 2007002/28					

SHEET

02

OF 10



MATCH LINE (SEE SHEET 02)

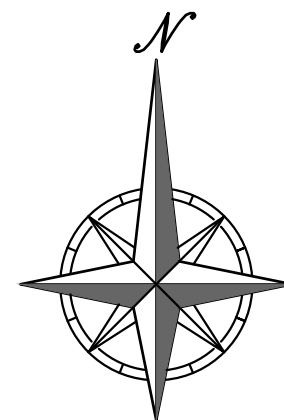
LOT 8  
PLAZA SOUTH

SITE KEY NOTES:

- (A) CONSTRUCT 2' CURB & GUTTER (TYPICAL).
- (B) CONSTRUCT PRIVATE CONCRETE SIDEWALK (TYPICAL).
- (C) CONSTRUCT PRIVATE SIDEWALK RAMP WITH LAYDOWN CURB & GUTTER.
- (D) CONSTRUCT ACCESSIBLE PARKING SPACE. INSTALL PAVEMENT MARKINGS, LAYDOWN CURB & BUMPERS. SEE ACCESSIBLE PARKING SPACE DETAIL.
- (E) INSTALL HANDICAPPED PARKING SIGN.
- (F) INSTALL VAN ACCESSIBLE PARKING SIGN.
- (G) CONSTRUCT SCORED CONCRETE PEDESTRIAN CROSSWALK.
- (H) INSTALL CONCRETE PAVEMENT.
- (I) INSTALL TRASH ENCLOSURE (REFER TO ARCH. PLANS).
- (J) INSTALL MONUMENT SIGN.
- (K) INSTALL PICNIC AREA.
- (L) CONSTRUCT CONCRETE PAD & INSTALL BENCH.
- (M) INSTALL BIKE RACK (5-STALLS).
- (O) INSTALL POLE SIGN.
- (P) INSTALL END OF ROAD MARKERS.

LEGEND

- PLAT LINE
- LOT LINE
- RIGHT-OF-WAY
- 2' CURB & GUTTER
- B.L. BUILDING SETBACK LINE
- P.L. PARKING SETBACK LINE
- L.S. LANDSCAPE SETBACK LINE
- HEAVY DUTY ASPHALT PAVEMENT
- ASPHALT PAVEMENT
- CONCRETE PAVEMENT
- CONCRETE SIDEWALK



SCALE: 1"=20'

W 188TH ST

PHELPS ENGINEERING, INC.  
1270 N. Winchester  
Olathe, Kansas 66061  
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Fax (913) 993-1166  
www.phelpsengineering.com

PLANNING  
ENGINEERING  
IMPLEMENTATION



SITE DIMENSION PLAN  
NEW LIFE COMMUNITY CHURCH  
GARDNER, KANSAS  
FINAL DEVELOPMENT PLANS

PROJECT NO.	181177	No.	Date	By	App.
DATE: 1/10/20	DRAWN: JMO	1	2-3-20	JMO	ALN
CHECKED: DEU	APPROVED:	2	2-11-20	JMO	ALN
CERTIFICATE OF AUTHORIZATION					
LAND SURVEYING - LS-82					
ENGINEERING - E-391					
CERTIFICATE OF AUTHORIZATION					
LAND SURVEYING - 2007001/28					
ENGINEERING - 2007002/28					

SHEET

03

OF 10

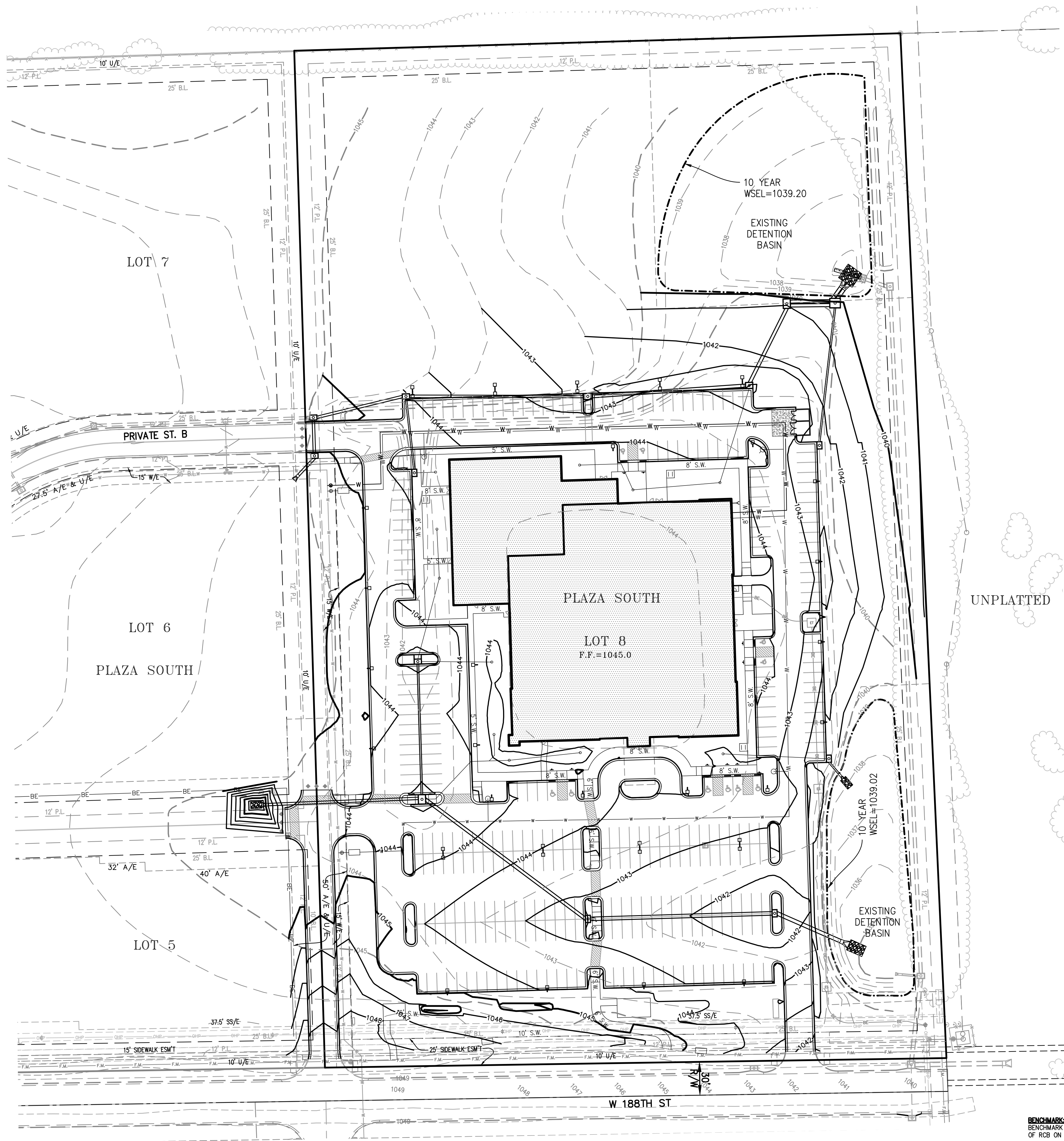


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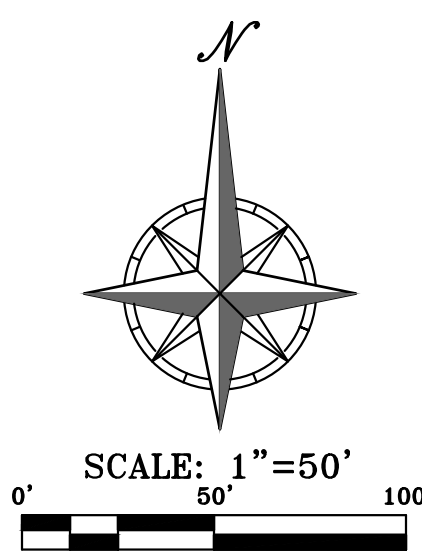
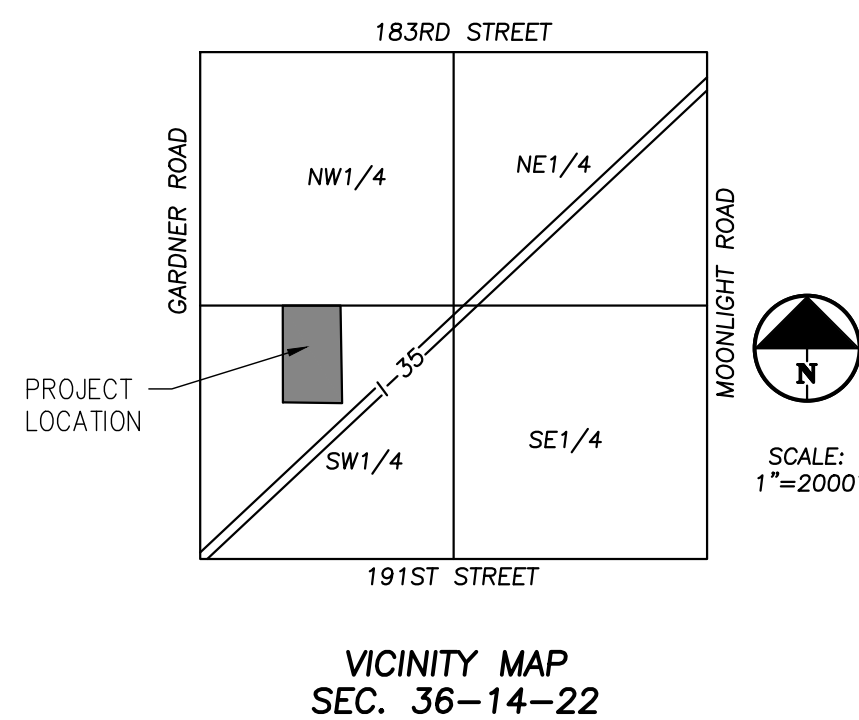
Know what's below.  
Call before you dig.

UTILITY NOTES:  
VISUAL INDICATIONS OF UTILITIES ARE AS SHOWN.  
UNDERGROUND LOCATIONS SHOWN, AS FURNISHED BY THEIR  
LESSORS, ARE APPROXIMATE AND SHOULD BE VERIFIED IN  
THE FIELD AT THE TIME OF CONSTRUCTION. FOR ACTUAL  
FIELD LOCATIONS OF UNDERGROUND UTILITIES CALL 811.



**NOTE:**

EXISTING CONTOURS SHOWN ARE A COMBINATION OF JOHNSON COUNTY AIM'S AND  
PLANNED ELEVATIONS PER MASS GRADING BY PLAZA SOUTH DEVELOPER. IT IS THE  
CONTRACTOR'S RESPONSIBILITY TO VERIFY THE EXISTING CONDITIONS PRIOR TO STARTING  
CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.



- BENCHMARK:** VERTICAL DATUM = NAVD88 BASED ON JOHNSON COUNTY VERTICAL DATUM JOHNSON COUNTY  
BENCHMARK NETWORK BERNSTEIN ALUMINUM DISK STAMPED #1106 IN EAST END OF THE NORTH HEADWALL  
OF RCB ON NORTH SIDE OF 191ST STREET. ELEVATION = 1025.79
- A. FOUND 4" CUT IN CENTER OF FRONT FACE OF CURB INLET ON EAST SIDE OF GARDNER ROAD AT  
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ELEVATION = 1044.12
- B. SET 4" CUT ON NORTHWEST CORNER OF CONCRETE PAD FOR ELECTRIC TRANSFORMER PAD AT  
NORTHEAST CORNER OF GROUNDHOUSE COFFEE.  
ELEVATION = 1046.77
- C. SET RAILROAD SPIKE WEST SIDE OF 5TH POWER POLE EAST OF GARDNER ROAD MIDDLE OF SOUTH  
PROPERTY.  
ELEVATION = 1050.08
- D. SET RAILROAD SPIKE WEST SIDE OF 8TH POWER POLE EAST OF GARDNER ROAD SOUTHEAST CORNER OF  
PROPERTY.  
ELEVATION = 1041.94

**FLOOD NOTE:**

THIS PROPERTY LIES WITHIN ZONE X, DEFINED AS AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL  
CHANCE FLOODPLAIN, AS SHOWN ON THE FLOOD INSURANCE RATE MAP PREPARED BY THE FEDERAL  
EMERGENCY MANAGEMENT AGENCY FOR THE CITY OF GARDNER, COMMUNITY NO. 200164, JOHNSON COUNTY,  
KANSAS, MAP NO. 20091001206, AND DATED AUGUST 3, 2009.

PHELPS ENGINEERING, INC  
1250 N. Winchester  
Olathe, Kansas 66061  
(913) 993-1155  
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GRADING PLAN  
NEW LIFE COMMUNITY CHURCH  
GARDNER, KANSAS  
FINAL DEVELOPMENT PLANS

PROJECT NO.	181177	No.	Date	Revisions:	By	App.
DATE: 1/10/20	DRAWN: JMO	1	2-3-20	REVISED PER CITY COMMENTS	JMO	ALN
CHECKED: DEU	APPROVED:	2	2-11-20	REVISED PER CITY COMMENTS	JMO	ALN
CERTIFICATE OF AUTHORIZATION						
LAND SURVEYING - LS-82						
LAND ENGINEERING - E-391						
CERTIFICATE OF AUTHORIZATION						
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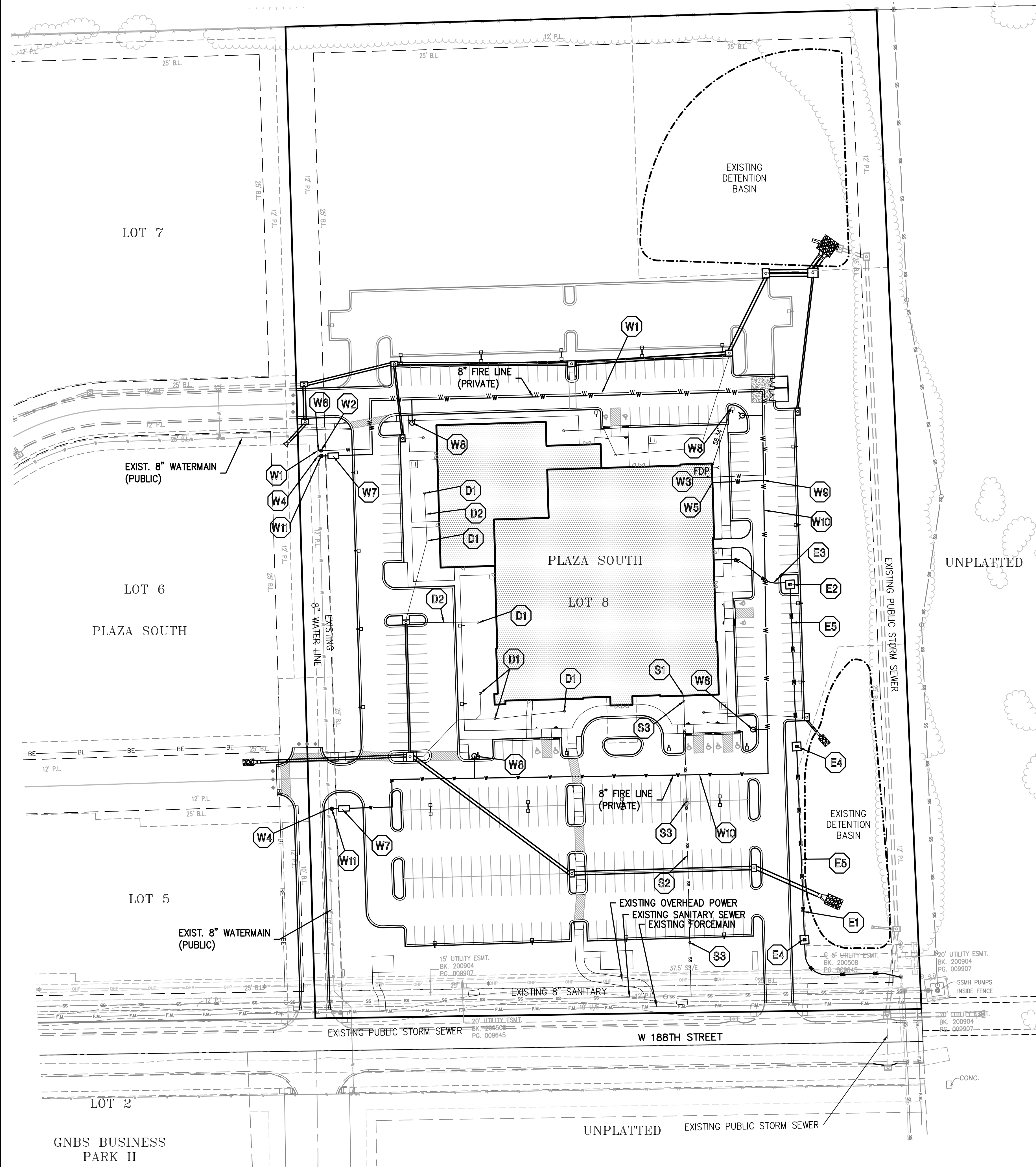
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OF 10



V:\PHELPS-SERVER\Projects\18117\18117.dwg Final Dev UTILITY PLAN.dwg Layout:1 Feb 14, 2020 - 11:42am Aaron Norris



#### KEY NOTES:

- W1** CONTRACTOR TO PERFORM AND COORDINATE 8"x2" TAP FOR PROPOSED BUILDING DOMESTIC SERVICE. CONTRACTOR TO INSTALL 2" TYPE K COPPER FROM TAP TO METER. CONTACT GARDNER WATER FOR TAPPING REQUIREMENTS. OWNER WILL REIMBURSE CONTRACTOR FOR METER OR SYSTEM DEVELOPMENT FEES BY CITY OF GARDNER.
- W2** PROVIDE AND INSTALL 2" WATER METER IN PIT PER GARDNER WATER REQUIREMENTS. THERE SHALL BE 2' OF CLEARANCE AROUND PIT AND THE PIT SHALL BE INSTALLED IN GREENSPACE. COORDINATE WITH GARDNER WATER FOR MAIN TAP. OWNER SHALL PAY ALL FEES FOR TAP AND METER. ALL LABOR AND MATERIALS SHALL BE PROVIDED AND INSTALLED BY THE CONTRACTOR'S PLUMBER IN ACCORDANCE WITH CITY OF GARDNER STANDARDS.
- W3** 3" DOMESTIC WATERLINE ENTRY TO BUILDING. CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING ANY APPURTENANCES ON THE DOMESTIC LINE SUCH AS BACKFLOW PREVENTION DEVICES (USC APPROVED RP2) (RE: BUILDING PLANS), GATE VALVES, REDUCERS, BENDS, TEES, ETC., WHICH MAY BE REQUIRED. CONTRACT TO COORDINATE WITH WATER UTILITY.
- W4** CONTRACTOR TO EXPOSE WATER MAIN AND COORDINATE FOR CITY OF GARDNER TO INSTALL 8" X 6" TAPPING SLEEVE AND VALVE FOR PROPOSED COMBINATION BUILDING DOMESTIC AND FIRE LINE. TAPPING SLEEVE SHALL BE 5' MINIMUM FROM ANY FITTINGS FOUND IN EXCAVATION. CONTACT CITY OF GARDNER FOR REQUIREMENTS. CONTRACTOR TO PAY ALL FEES FOR WATER MAIN TAP. OWNER WILL REIMBURSE CONTRACTOR FOR FEES BY CITY OF GARDNER.
- W5** 6" SPRINKLER ENTRY (RE: ARCH.) CONTRACTOR SHALL BE REQUIRED TO INSTALL ANY APPURTENANCES ON THE SPRINKLER LINE SUCH AS, BUT NOT LIMITED TO, A USC APPROVED DOUBLE CHECK VALVE BACKFLOW PREVENTION ASSEMBLY, GATE VALVES, REDUCERS, BENDS, TEES, ETC., MEETING CITY FIRE DEPT. AND WATER DEPT. CRITERIA.
- W6** CONTRACTOR INSTALL 2" TYPE K COPPER DOMESTIC SERVICE LINE FROM COMBINATION LINE TAP TO WATER METER. SERVICE LINE SHALL RUN PERPENDICULAR TO COMBINATION LINE WITH NO FITTINGS BETWEEN THE TAP AND THE METER SETTER.
- W7** INSTALL BACK FLOW PREVENTION DEVICE (DOUBLE CHECK) IN VAULT ON PROPOSED 8" WATER LINE
- W8** INSTALL PRIVATE RED FIRE HYDRANT ASSEMBLY
- W9** INSTALL 8"x6" TEE
- W10** INSTALL 8" PVC (C-900)
- W11** INSTALL 8" GATE VALVE
- E1** INSTALL 4" PVC CONDUIT FROM EXISTING POWER POLE TO TRANSFORMER
- E2** PROPOSED LOCATION OF CONCRETE TRANSFORMER PAD. CONTRACTOR TO VERIFY EXACT LOCATION AND SIZE WITH GARDNER ELECTRIC PRIOR TO CONSTRUCTION. CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION AND COST OF CONCRETE PAD AND CONDUIT AS REQUIRED BY THE ELECTRIC COMPANY. CONTRACTOR SHALL COORDINATE SAID WORK WITH THE ELECTRIC COMPANY.
- E3** INSTALL 4" PVC CONDUIT FOR SECONDARY POWER TO BUILDING. CONTRACTOR IS RESPONSIBLE FOR THE COST AND INSTALLATION OF THE SERVICE CABLES FROM THE TRANSFORMER TO THE SERVICE ENTRANCE.
- E4** PROPOSED LOCATION OF ELECTRIC CABINET. CONTRACTOR TO VERIFY EXACT LOCATION AND SIZE WITH GARDNER ELECTRIC PRIOR TO CONSTRUCTION. CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF ELECTRIC CABINET AND CONDUIT AS REQUIRED BY THE ELECTRIC COMPANY. CONTRACTOR SHALL COORDINATE SAID WORK WITH THE ELECTRIC COMPANY.
- E5** CONTRACTOR TO INSTALL 4" PVC (SCHEDULE 40) CONDUIT (HERITAGE PLASTICS OR CANTEX) FROM POLE TO THE CABINET AND TO THE TRANSFORMER (42"-48" DEPTH) CONTRACTOR TO COORDINATE WITH CITY OF GARDNER FOR ACCEPTANCE INSPECTION.
- S1** 6" SANITARY SERVICE EXIT FROM BUILDING
- S2** CONTRACTOR TO INSTALL 6" PVC (SDR-26) W/RUBBER GASKETED JOINTS SANITARY SERVICE LINE & CONNECT TO EXISTING SANITARY SEWER MAIN.
- D1** INSTALL 18" NYLOPLAST DRAIN BASIN
- D2** TYPICAL STORM WATER DRAIN LINE FORM ROOF DRAINS TO STORM SEWER. (SEE ARCH. PLANS FOR BUILDING CONNECTION LOCATIONS)

#### UTILITY COMPANIES:

GARDNER ELECTRIC DISTRIBUTION MANAGER  
MR. BRUCE BALDWIN (BBALDWIN@GARDNERKANSAS.GOV)  
1450 E. SANTA FE STREET  
GARDNER, KANSAS 66030  
(913) 856-0985 (EXT. 2985)

KANSAS GAS SERVICE CO.  
MR. DAVID TEEFEY (DAVID.TEEFEY@onegas.com)  
11401 W. 89TH ST.  
OVERLAND PARK, KANSAS 66214  
(913) 599-8933

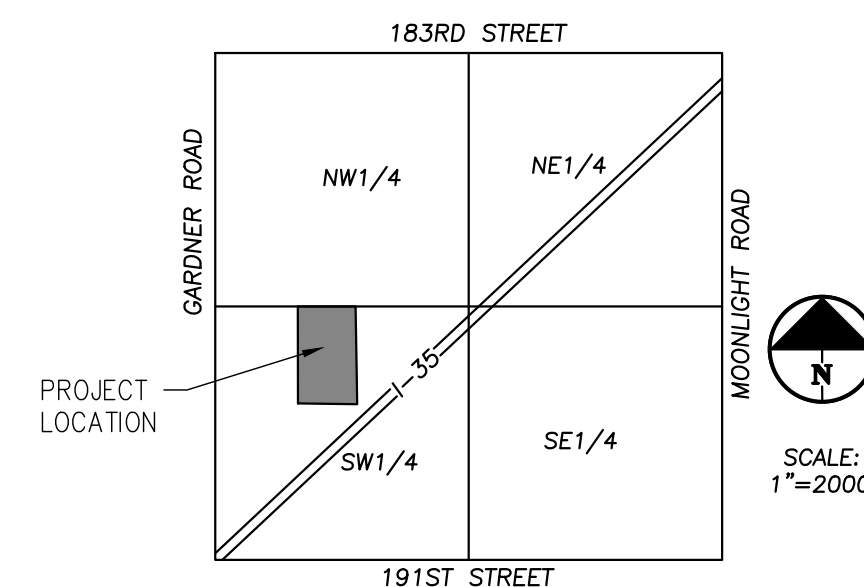
TIME WARNER CABLE (SPECTRUM)  
MR. STEVEN BAXTER (steven.baxter@charter.com)  
8221 W. 119TH STREET  
OVERLAND PARK, KANSAS 66213  
(913) 643-1901  
(913) 451-7652 FAX

CITY OF GARDNER  
SANITARY SEWER & WATER UTILITIES DEPARTMENT  
913-856-0980

KANSAS FIBER NET  
BRAD BURGER  
913-213-2937

#### FLOOD NOTE:

THIS PROPERTY LIES WITHIN ZONE X, DEFINED AS AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN, AS SHOWN ON THE FLOOD INSURANCE RATE MAP PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY FOR THE CITY OF GARDNER, COMMUNITY NO. 200164, JOHNSON COUNTY, KANSAS, MAP NO. 2009100206, AND DATED AUGUST 3, 2009.



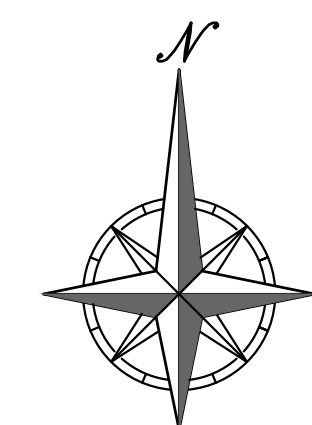
VICINITY MAP  
SEC. 36-14-22

#### EXISTING LEGEND

SS	SS	SANITARY SEWER
STM	STM	STORM SEWER
BT	BT	BURIED TELEPHONE
FO	FO	FIBER OPTIC LINE
G	G	GAS LINE
BE	BE	BURIED ELECTRIC LINE
CHP	CHP	OVERHEAD POWER LINE
SAH, MH	SAH, MH	SANITARY SEWER MANHOLE
SS	SS	STREET SIGN
STLB	STLB	STREET LIGHT BOX
STM MH	STM MH	STORM SEWER MANHOLE
TCB	TCB	TRAFFIC CONTROL BOX
TR	TR	TELEPHONE RISER
TVR	TVR	TELEVISION RISER
TSL	TSL	TRAFFIC SIGNAL LIGHT
TSLB	TSLB	TRAFFIC SIGNAL LIGHT BOX
WV	WV	WATER VALVE
BTM	BTM	BURIED TELEPHONE MARKER
EM	EM	ELECTRIC METER
ER	ER	ELECTRIC RISER
FA	FA	FIRE HYDRANT
GA	GA	GUY ANCHOR
GP	GP	GUARD POST
GM	GM	GAS METER
GV	GV	GAS VALVE
LP	LP	LIGHT POLE
PP	PP	POWER POLE

#### PROPOSED LEGEND

SS	SS	SANITARY SEWER LINE
STM	STM	STORM SEWER LINE
BT	BT	BURIED TELEPHONE LINE
WV	WV	WATER LINE
BE	BE	BURIED ELECTRIC LINE



SCALE: 1"=50'  
0' 50' 100'



Know what's below.  
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**UTILITY NOTES:**  
VISUAL INDICATIONS OF UTILITIES ARE AS SHOWN. UNDERGROUND LOCATIONS SHOWN, AS FURNISHED BY THEIR LESSORS, ARE APPROXIMATE AND SHOULD BE VERIFIED IN THE FIELD AT THE TIME OF CONSTRUCTION. FOR ACTUAL FIELD LOCATIONS OF UNDERGROUND UTILITIES CALL 811.

**BENCHMARK:** VERTICAL DATUM = NAVD88 BASED ON JOHNSON COUNTY VERTICAL DATUM JOHNSON COUNTY BENCHMARK NETWORK BERNSTEIN ALUMINUM DISK STAMPED #106 IN EAST END OF THE NORTH HEADWALL OF RCB ON NORTH SIDE OF 191ST STREET. ELEVATION = 1025.79

A. FOUND 4" CUT IN CENTER OF FRONT FACE OF CURB INLET ON EAST SIDE OF GARDNER ROAD AT MIDDLE OF PHILLIPS 66.  
ELEVATION = 1044.12

B. SET 4" CUT ON NORTHWEST CORNER OF CONCRETE PAD FOR ELECTRIC TRANSFORMER PAD AT NORTHEAST CORNER OF GROUNDHOUSE COFFEE.  
ELEVATION = 1046.77

C. SET RAILROAD SPIKE WEST SIDE OF 5TH POWER POLE EAST OF GARDNER ROAD MIDDLE OF SOUTH PROPERTY.  
ELEVATION = 1050.08

D. SET RAILROAD SPIKE WEST SIDE OF 8TH POWER POLE EAST OF GARDNER ROAD SOUTHEAST CORNER OF PROPERTY.  
ELEVATION = 1041.94

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UTILITY PLAN  
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GARDNER, KANSAS  
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ENGINEERING	E-361		
CERTIFICATE OF AUTHORIZATION	202001028		
LAND SURVEYING	202001028		
ENGINEERING	202002028		

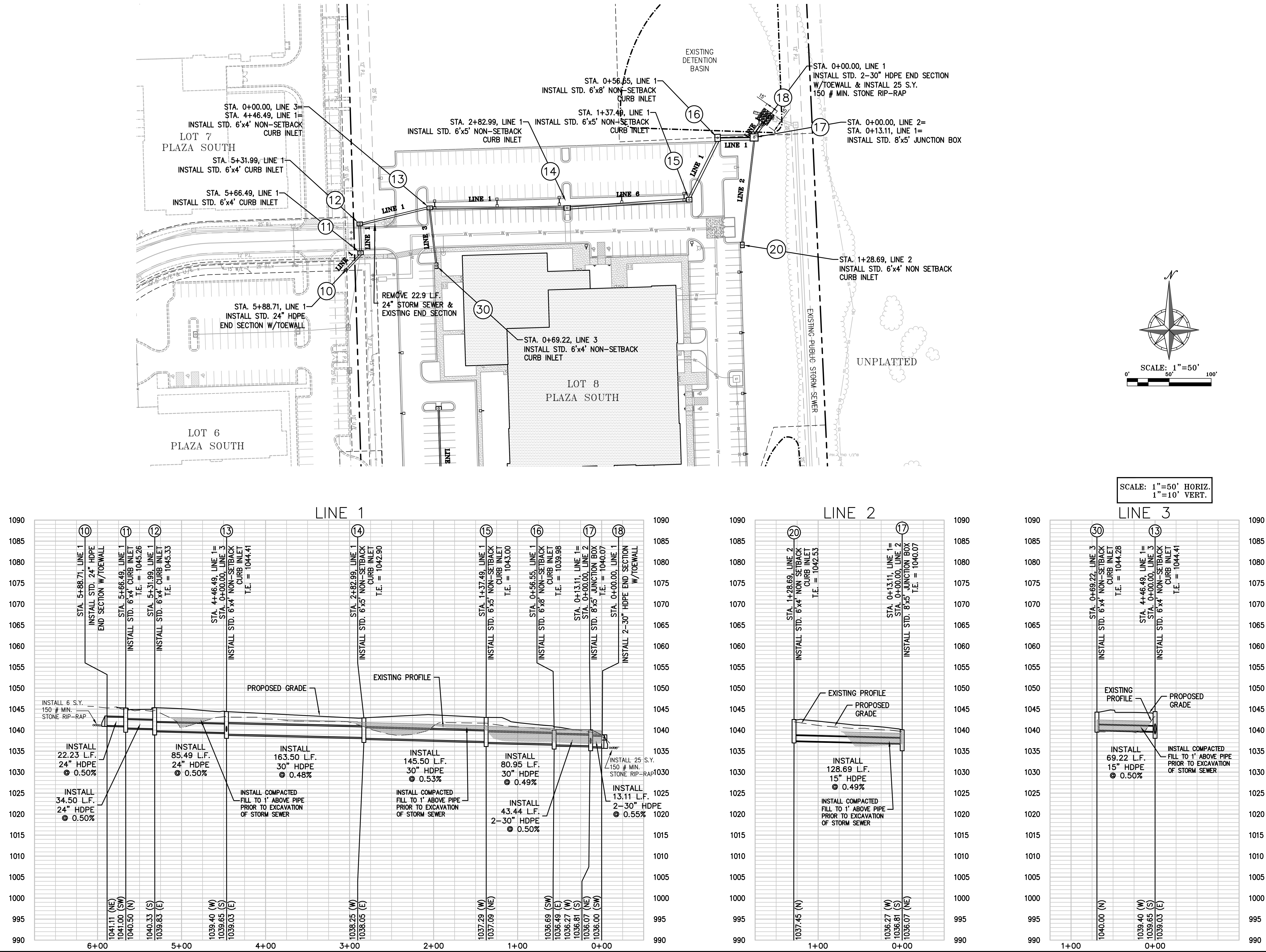
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\\PHELPS-SERVER\Projects\181177\Drawings\181177\Storm Sewer Profiles.dwg Layout:1 Feb 14, 2020 11:42am Aaron Norris



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**STORM SEWER PLAN & PROFILE**  
NEW LIFE COMMUNITY CHURCH  
GARDNER, KANSAS  
FINAL DEVELOPMENT PLANS

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ENGINEERING - E-391						
CERTIFICATE OF AUTHORIZATION						
LAND SURVEYING - 200700128						
ENGINEERING - 200700328						

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OF 10












\\PHELPS-SERVER\Projects\181177\dwg\Final Dev\DRAINAGE MAP.dwg    Layout:2    Feb 14, 2020    11:42am    Aaron Norris

STORM DRAINAGE CALCULATIONS

DESIGN CRITERIA:    K25 = 1.1; K100 = 1.25; n = 0.013 (RCP); STORM FREQUENCY = 100 YEAR; A.I.= AREA INLET; J.B.= JUNCTION BOX; C.I. = CURB INLET; C.C. = CURB CUT; G.I. = GRATE INLET; HEIGHT OF STRUCTURE=RIM ELEV MINUS FLOWLINE OUT .																									
I. RUNOFF												III. PIPE DESIGN												REMARKS	
N U M B E R	S T R U C T U R E	INCREMENTAL			CUMULATIVE		SYSTEM TIME OF CONCENTRATION "T <sub>c</sub> " AT STRUCTURE (MIN)	RAINFALL INTENSITY "I <sub>25</sub> " / I <sub>100</sub> " (IN/HR)	ANTECEDENT PRECIPITATION FACTOR "K <sub>25</sub> " / K <sub>100</sub> "	RUNOFF "Q <sub>25</sub> " / Q <sub>100</sub> " (CFS)	STRUCTURE				PIPE										
		RUNOFF COEFFICIENT "C"	AREA "A" (ACRES)	C x A	AREA "A" (ACRES)	C x A					Upstream Structure Number	Downstream Structure Number	Upstream Structure Rim Elevation	Height of Structure (FT)	Diameter "D" (IN)	Length "L" (FT)	Upstream Invert Elevation	Downstream Invert Elevation	Slope "S" (FT/FT)	Travel Time in Pipe "TT" (min)	Velocity Full V <sub>f</sub> (FPS)	Runoff Q <sub>25</sub> (CFS)	Runoff Q <sub>100</sub> (CFS)	Full Flow Q <sub>f</sub> (CFS)	
1	10	0.81	1.30	1.05	1.30	1.05	5.00	8.53	1.10	9.9	10	11	1045.00	3.89	24	22.23	1041.11	1041.00	0.0049	0.07	5.1	9.9	13.5	15.9	
								10.32	1.25	13.5															
	11	0.81	0.00	0.00	1.30	1.05	5.07	8.50	1.10	9.8	11	12	1045.00	4.50	24	34.50	1040.50	1040.33	0.0049	0.11	5.1	9.8	13.5	15.9	
								10.29	1.25	13.5															
	12	0.81	0.28	0.23	1.58	1.28	5.19	8.46	1.10	11.9	12	13	1045.00	5.17	24	85.49	1039.83	1039.40	0.0050	0.28	5.1	11.9	16.4	16.0	
								10.25	1.25	16.4															
	13	0.81	0.00	0.00	1.58	1.52	5.46	8.37	1.10	14.0	13	14	1044.37	5.34	30	156.00	1039.03	1038.25	0.0050	0.44	5.9	14.0	19.3	29.0	Add Line 3
								10.13	1.25	19.3															
	14	0.81	0.82	0.66	2.40	2.18	5.90	8.22	1.10	19.7	14	15	1043.21	5.16	30	153.00	1038.05	1037.29	0.0050	0.43	5.9	19.7	27.1	28.9	
2								9.96	1.25	27.1															
	15	0.81	0.24	0.19	2.64	2.37	6.33	8.08	1.10	21.1	15	16	1043.00	5.91	30	79.61	1037.09	1036.69	0.0050	0.22	5.9	21.1	29.0	29.1	
								9.80	1.25	29.0															
	16	0.81	0.93	0.75	3.57	3.12	6.56	8.01	1.10	27.5	16	17	1040.16	3.67	36	43.39	1036.49	1036.27	0.0051	0.11	6.7	27.5	37.9	47.5	2-30"
								9.71	1.25	37.9															
	17	0.81	0.00	0.00	3.57	3.35	6.66	7.97	1.10	29.4	17	18	1039.39	3.32	36	14.39	1036.07	1036.00	0.0049	0.04	6.6	29.4	40.5	46.5	2-30"
								9.67	1.25	40.5															Add Line 2
	20	0.81	0.28	0.23	0.28	0.23	5.00	8.53	1.10	2.2	20	17	1043.00	6.07	15	121.88	1036.93	1036.32	0.0050	0.54	3.8	2.2	3.0	4.6	
								10.32	1.25	3.0															
3	30	0.81	0.30	0.24	0.30	0.24	5.00	8.53	1.10	2.3	30	13	1044.00	4.00	15	69.22	1040.00	1039.65	0.0051	0.31	3.8	2.3	3.1	4.6	
							10.32	1.25	3.1																
4	40	0.81	0.50	0.41	0.50	0.41	5.00	8.53	1.10	3.8	40	41	1041.79	3.96	18	15.95	1037.83	1037.75	0.0050	0.06	4.2	3.8	5.3	7.4	
								10.32	1.25	5.3															
5	50	0.81	0.76	0.62	0.76	0.62	5.00	8.53	1.10	5.8	50	51	1043.10	4.67	18	124.00	1038.43	1037.68	0.0060	0.44	4.7	5.8	8.0	8.2	
								10.32	1.25	8.0															
	51	0.81	3.14	2.54	3.90	3.16	5.44	8.37	1.10	29.1	51	52	1043.00	5.52	36	184.01	1037.48	1036.56	0.0050	0.46	6.7	29.1	40.1	47.2	
								10.14	1.25	40.1															
	52	0.81	0.84	0.68	4.74	3.84	5.90	8.22	1.10	34.7	52	53	1042.60	6.24	42	168.03	1036.36	1035.52	0.0050	0.38	7.4	34.7	47.8	71.1	
								9.96	1.25	47.8															
6	53	0.81	1.34	1.09	6.08	4.93	6.28	8.09	1.10	43.9	53	54	1042.00	6.68	42	64.45	1035.32	1035.00	0.0050	0.15	7.4	43.9	60.5	70.9	
								9.82	1.25	60.5															
	60	0.81	1.61	1.30	1.61	1.30	5.00	8.53	1.10	12.2	60	51	1041.79	3.42	30	138.07	1038.37	1037.68	0.0050	0.39	5.9	12.2	16.8	29.0	
								10.32	1.25	16.8															

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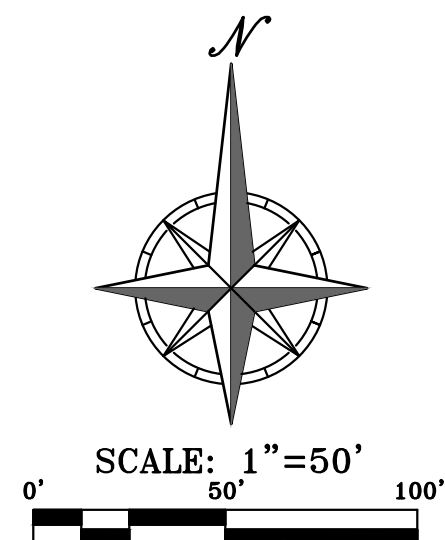
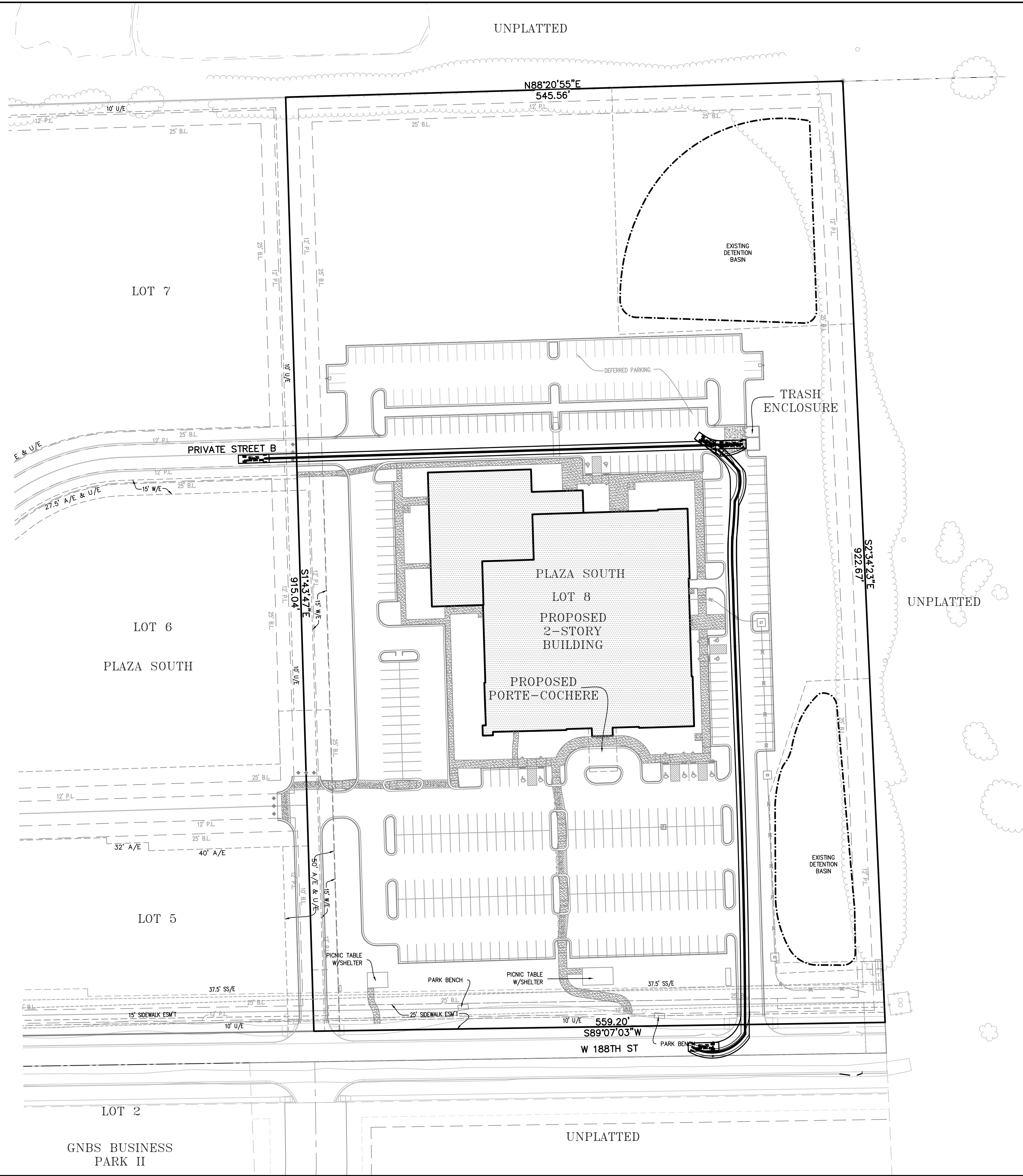
DRAINAGE CALCULATIONS  
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\\PHELPS-SERVER\Projects\181177\dwg\Final Dev\TRUCK MOVEMENT.dwg Layout:1 Feb 14, 2020 - 11:42am Aaron Morris



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**TRUCK MOVEMENT**  
**NEW LIFE COMMUNITY CHURCH**  
**GARDNER, KANSAS**  
**FINAL DEVELOPMENT PLANS**

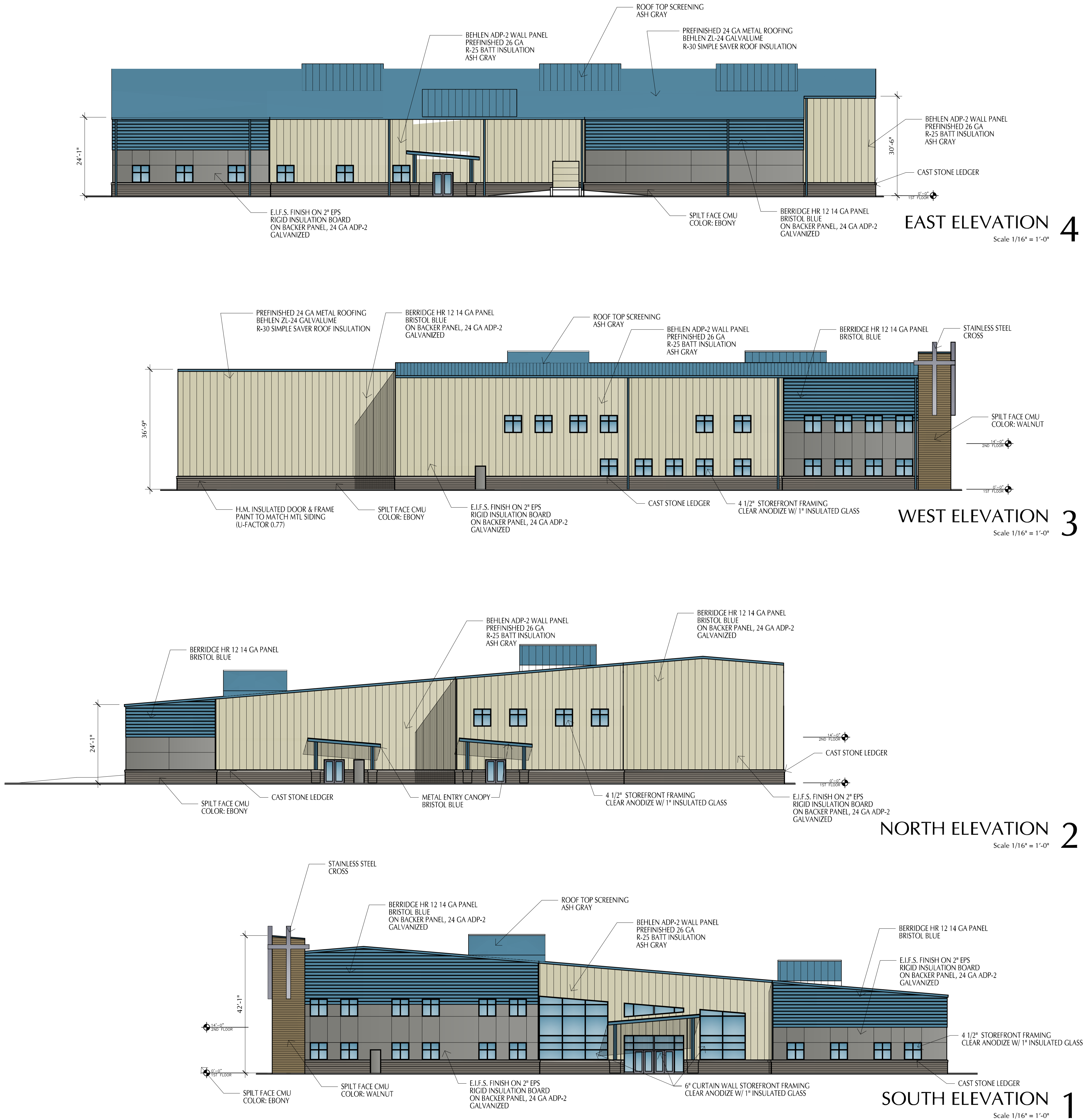


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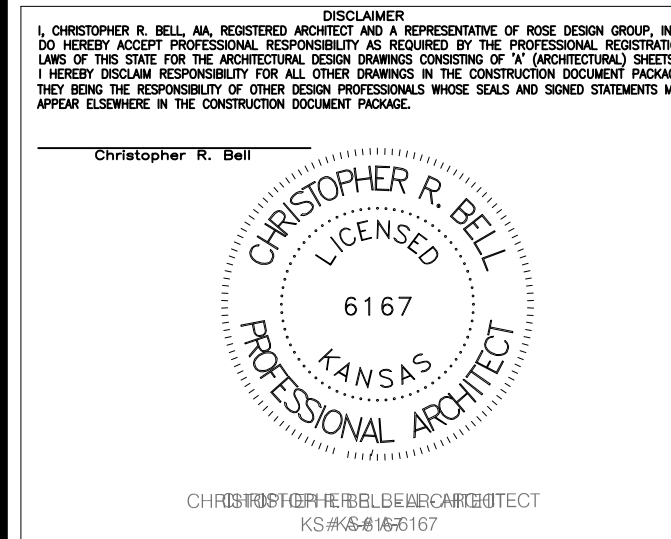
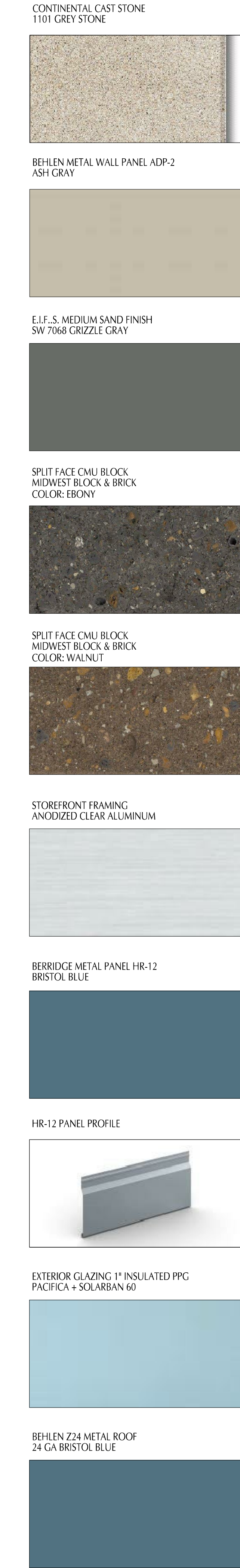
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T: \\Rose\\Drawings-Current\\17043 New Life Community Church\\Production\\Construction Documents\\Architecture\\A3.1 PHASE I BUILDING ELEVATIONS.dwg  
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## NEW BUILDING FOR: NEW LIFE COMMUNITY CHURCH LOT 8 PLAZA SOUTH DEVELOPMENT GARDNER, JOHNSON COUNTY, KANSAS

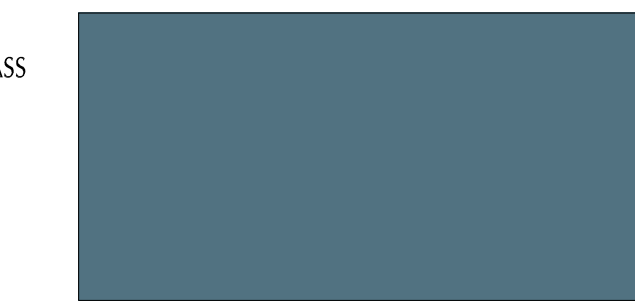
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PROJECT NUMBER 17043  
DATE ISSUED: 03 / 01 / 20

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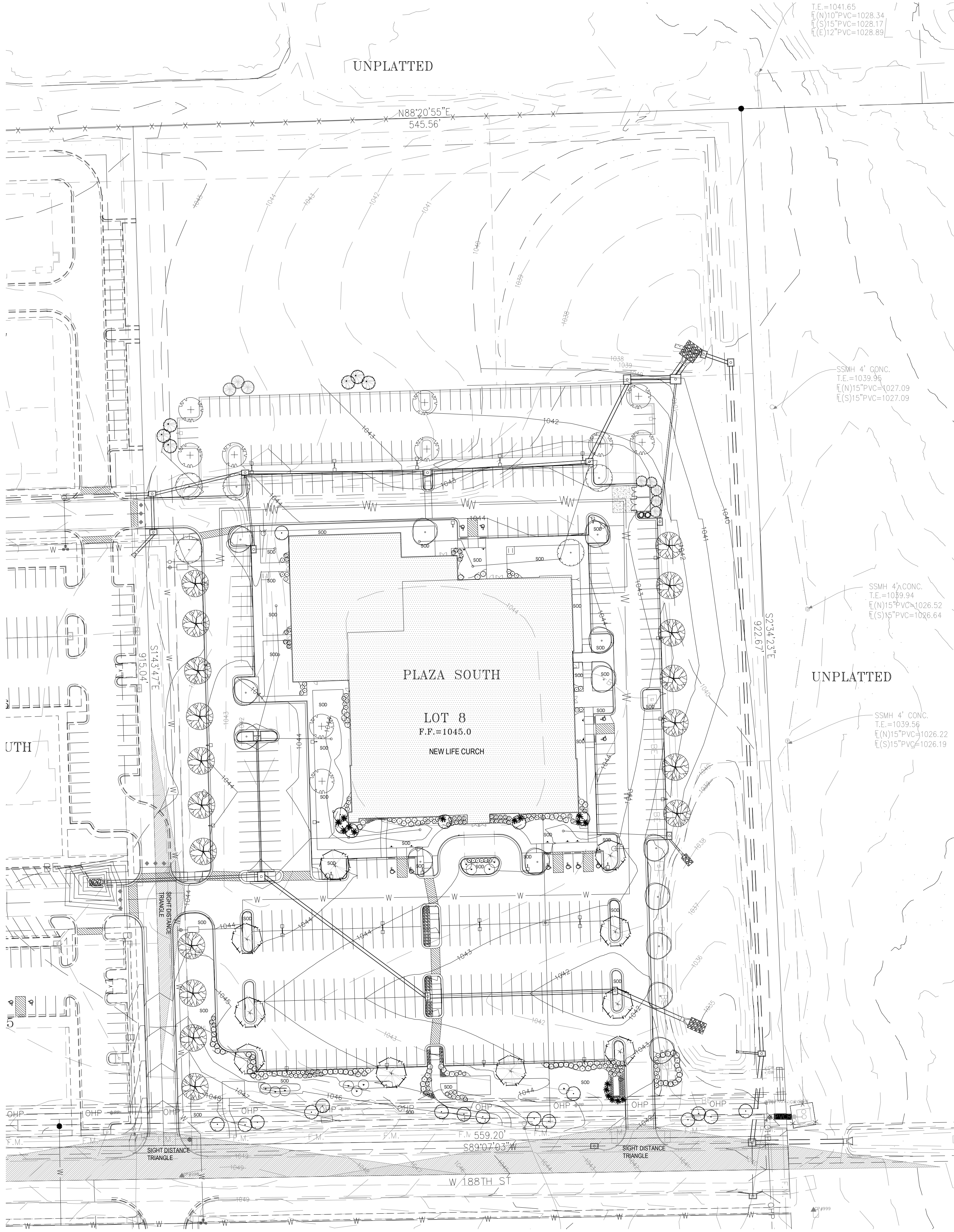
PHASE I  
BUILDING ELEVATIONS





## BUILDING ELEVATIONS





# 1 OVERALL SITE LANDSCAPE PLAN

SCALE: 1"=50'-0"

## Planting Notes

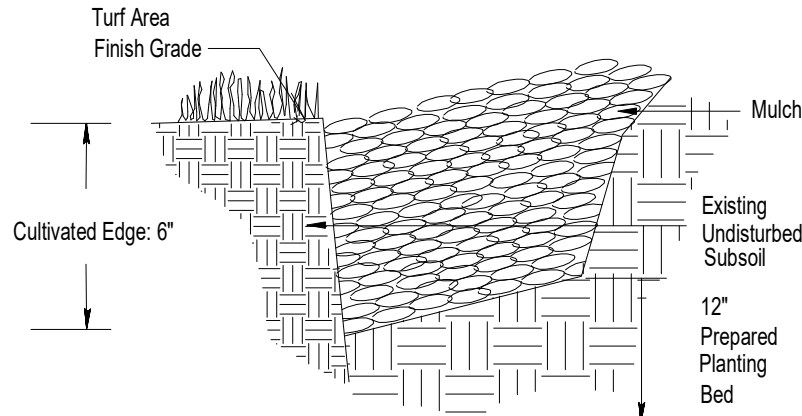
1. Location of all existing utilities needs to be done before commencing work.
2. The planting plan graphically illustrates overall plant massings. Each plant species massing shall be placed in the field to utilize the greatest coverage of ground plane. The following applies for individual plantings:
  - a. Creeping groundcover shall be a minimum of 6" from paving edge.
  - b. All trees shall be a minimum of 3' from paving edge.
  - c. All plants of the same species shall be equally spaced apart and placed for best aesthetic viewing.
  - d. All shrubs shall be a minimum of 2' from paved edge.
3. Mulch all planting bed areas to a minimum depth of 3". Mulch individual trees to a minimum depth of 4".
4. Note: If plants are not labeled - they are existing and shall remain.
5. In the event of work in or on a JCW sanitary main, any trees or plantings placed within the sewer easement may be removed without replacement or compensation there-of and shall be replaced by the property owner as required by the City.
6. All landscaped areas in ROW shall be sodded and irrigated unless otherwise specified.
7. Plant trees and shrubs 10' from the front and 3' from the sides of cabinet and transformer.
8. All electrical and mechanical equipment such as transformers, air conditioners, or communication equipment shall be screened form streets or adjacent lots.

### Materials:

1. Plant material shall be healthy, vigorous, and free of disease and insects as per AAN standards.
2. Shredded bark mulch installed at trees shall be finely chipped and shredded hardwood chips, consisting of pure wood products and free of all other foreign substances. Fine bark compost mulch installed at planting bed areas shall be free of all other foreign substances.

### Installation:
























1. All planting beds shall be amended with 1 cubic yard of peat moss per 1,000 square feet. Till peat moss into soil to a 6" depth. A 10-10-10 fertilizer shall be spread over all planting areas prior to planting, at a rate of 50 pounds per 2,000 square feet.
2. After plants have been installed, all planting beds shall be treated with Dacthal pre-emergent herbicide prior to mulch application.
3. Plant pit backfill for trees and shrubs shall be 50% peat or well composted manure and 50% topsoil.
4. Plant material shall be maintained and guaranteed for a period of one year after Owner's acceptance of finished job. All dead or damaged plant material shall be replaced at Landscape Contractor's expense.
6. Landscape contractor shall maintain all plant material until final acceptance, at which point the one year guarantee begins.



## 2 CULTIVATED EDGE DETAIL

SCALE: NTS

## Landscape Schedule

Symbol	Qty.	%	Botanal Name	Common Name	Min.Root	Min.Size	Caliper	Remarks	
OVERSTORY TREES									
	10	14	Gleditsia triacanthos 'Skyline'	Shademaster Honeylocust		2"	6' min. clear., ground to canopy		
	10	14	Quercus rubra	Northern Red Oak		2"	6' min. clear., ground to canopy		
	14	21	Acer x freemanii 'Autumn Blaze'	Autumn Blaze Maple		2"	6' min. clear., ground to canopy		
	18	26	Ulmus parvifolia	Laobark Elm		2"	6' min. clear., ground to canopy		
	17	25	Acer x truncatum 'Warrenred'	Pacific Sunset Maple		2"	6' min. clear., ground to canopy		
	69	100	TOTAL SHADE TREES ON SITE						
EVERGREEN TREES									
	5		Juniperus chinensis 'Keteleeri'	Keteleeri Juniper		6' ht.		symmetrical pyramidal form	
	3		Juniper chinensis	Eastern Red Cedar		6' Ht.		symmetrical pyramidal form	
	6		Picea abies	Norway Spruce		6' ht.		symmetrical pyramidal form	
	14		TOTAL EVERGREEN TREES ON SITE						
ORNAMENTAL TREES									
	22		Cercis canadensis	Eastern Redbud		1.5"			
	11		Cornus florida 'Cloud Nine'	Cloud 9 Dogwood		1.5"			
	33		TOTAL ORNAMENTAL TREES ON SITE						
DECIDUOUS SHRUBS/GRASSES									
	95		Liriope muscari 'Variegated'	Variegated Liriope	1 gal.			Plant @ 18" O.C.	
	7		Spiraea x bumalda 'Goldflame'	Goldflame spiraea	3 gal.			Plant @ 3' O.C.	
	57		Hydrangea paniculata 'Quick Fire'	Little Quick Fire Hydrangea	3 gal.			Plant @ 4' O.C.	
	51		Syringa X 'Penda'	Bloomerang Purple Lilac	5 gal.			Plant @ 5' O.C.	
EVERGREEN SHRUBS									
	42		Juniperus chinensis 'Sea Green'	Sea Green Juniper	3 gal.			Plant @ 4' O.C.	
	77		Juniperus chinensis 'Gold Coast'	Gold Coast Juniper	3 gal.			Plant @ 4' O.C.	
	2		Juniperus chinensis 'Spartan'	Spartan Juniper		5' ht.		Symmetrical pyramidal form	
GROUNDCOVERS AND GRASSES									
	17		Juniperus horizontalis 'Wiltonii'	Blue Rug Juniper	1 gal.			Plant @ 24" O.C.	
	56		Pennisetum alopecuroides 'Hameln'	Dwarf Fountain Grass	1 qt.			Plant @ 18" O.C.	
	404		TOTAL SHRUBS/GRASSES ON SITE						

### TREE PLANTING NOTES:

- 1) DO NOT HEAVILY PRUNE THE TREE. PRUNE ONLY CROSSOVER LIMBS CO-DOMINANT LEADERS, & BROKEN OR DEAD BRANCHES SOME INTERIOR TWIGS & LATERAL BRANCHES MAY BE PRUNED. DO NOT REMOVE THE TERMINAL BUDS OF BRANCHES THAT EXTEND TO THE EDGE OF THE CROWN
- 2) MARK THE NORTH SIDE OF THE TREE IN THE NURSERY, AND ROTATE TREE TO FACE NORTH AT THE SITE WHENEVER POSSIBLE
- 3) SET TOP OF ROOT BALL 1-2 INCHES HIGHER THAN SURROUNDING GRADE
- 4) APPLY 4"THK WOOD MULCH, DO NOT PLACE MULCH IN DIRECT CONTACT w/ TREE TRUNK
- 5) EACH TREE MUST BE PLANTED SUCH THE TRUNK FLARE IS VISIBLE AT THE TOP OF THE ROOT BALL. TREES WHERE THE FLARE IS NOT VISIBLE SHALL BE REJECTED. DO NOT COVER THE TOP OF THE ROOT BALL w/ SOIL
- 6) REMOVE ALL TWINE, ROPE, WIRE AND BURLAP FROM THE UPPER 1/3 OF ROOT BALL (REMOVE WIRE BASKETS)
- 7) PLACE ALL ROOT BALLS ON UN-COMPACTED OR TAMPED SOIL, TYP

### STAKING REQUIREMENTS:

- 1) WIRE / CABLE SHALL BE GALV, 12-GAUGE
- 2) TIGHTEN WIRE / CABLE ONLY ENOUGH TO KEEP FROM SLIPPING. ALLOW FOR SOME TRUNK MOVEMENT PLASTIC HOSE SHALL BE LONG ENOUGH TO ACCOMMODATE 1/2" OF GROWTH
- 3) STAKES SHALL BE 2"x 2" HARDWOOD OR EQUAL

### PERENNIAL PLANTING NOTES:

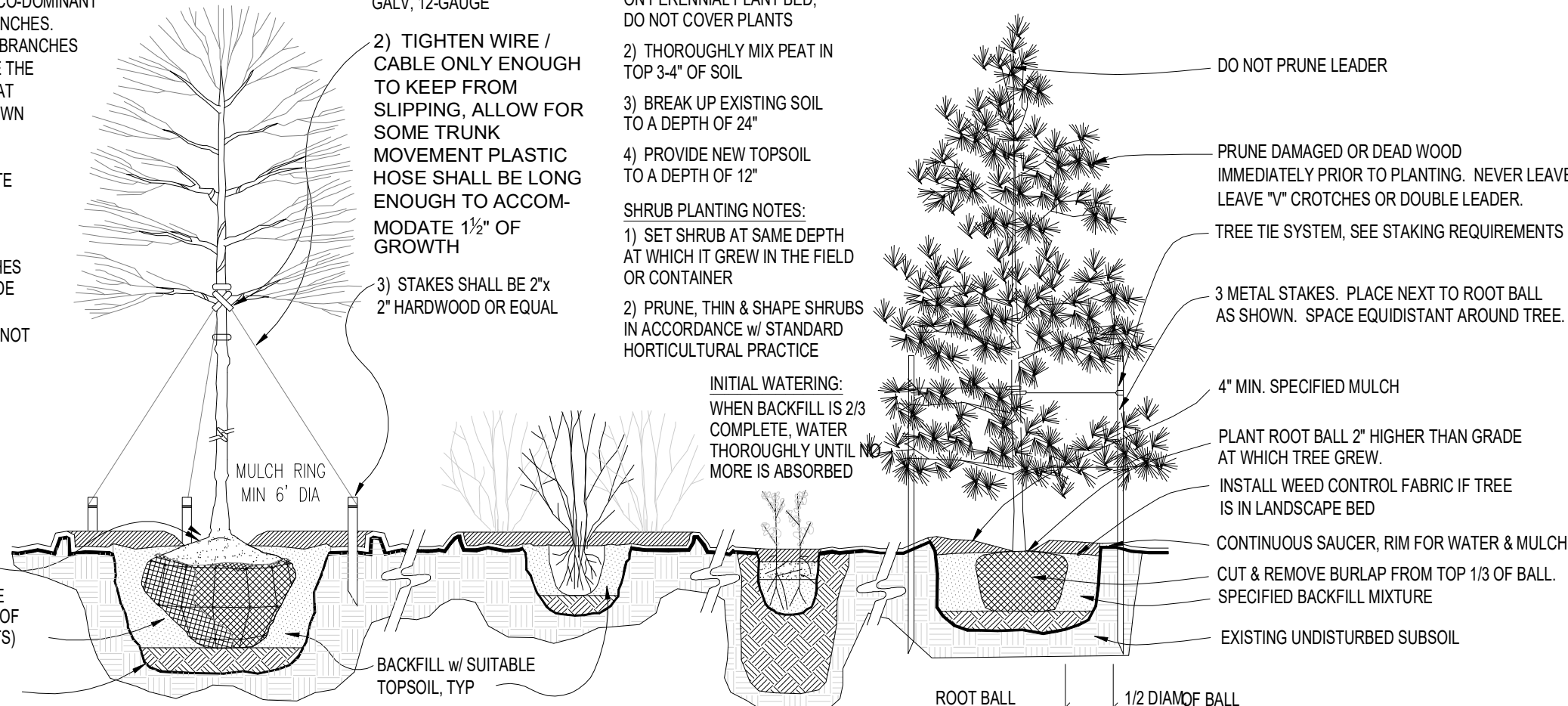
- 1) APPLY 2"THK BED OF MULCH ON PERENNIAL PLANT BED. DO NOT COVER PLANTS
- 2) THOROUGHLY MIX PEAT IN TOP 3-4" OF SOIL
- 3) BREAK UP EXISTING SOIL TO A DEPTH OF 24"
- 4) PROVIDE NEW TOPSOIL TO A DEPTH OF 12"

### SHRUB PLANTING NOTES:

- 1) SET SHRUB AT SAME DEPTH AT WHICH IT GREW IN THE FIELD OR CONTAINER
- 2) PRUNE, THIN & SHAPE SHRUBS IN ACCORDANCE w/ STANDARD HORTICULTURAL PRACTICES

### INITIAL WATERING:

- 1) WHEN BACKFILL IS 2/3 COMPLETE, WATER THOROUGHLY UNTIL NO MORE IS ABSORBED



## 3 PLANTING INSTALLATION DETAILS

SCALE: NTS

## Landscape Calculations - Non-Residential

### Street Trees - 40' max. on center along public street frontage (Excluding Driveways):

- Entry Road = 206 LF. 5 Trees required. 5 Trees provided.  
188th street has overhead power lines, so small trees need to be planted at 25' on center.  
188th Street = 541 LF. 22 Small Trees required. 22 Small Trees provided.

### Parking Area Landscaping:

- 1 large or medium tree for each 40 spaces. 402 spaces provided - 10 trees required.  
1 shrub for every 5 linear feet of perimeter buffer along frontage/street side.  
409 L.F. Parking Lot Perimeter. 82 shrubs required. 82 shrubs provided.  
1 large or medium tree for every 50 linear feet of perimeter buffer.  
409 L.F. Parking Lot Perimeter. 8 trees required. 8 trees provided.  
50% reduction if designed with 2.5' - 4' decorative wall or fence complementing the architecture and materials of the building along the street side. (none proposed)

### Foundation Plantings:

- Except for buildings permitted within 10' of ROW, foundation planting areas shall exist along at least 25% of street facing facades and be at least 8' deep.  
1 small tree for every 40 LF of foundation. 207 LF of foundation - 6 Small trees Required.  
1 shrub for every 10 LF of foundation. 207 LF of foundation - 21 Shrubs Required.

### Other Open Space Areas:

- 1 tree per 10,000 s.f. 267,114sf 28 Trees Required 28 Trees Provided  
1 shrub per 5,000 s.f. 267,114sf 53 Shrubs Required 53 Shrubs Provided

### Buffer Planting Requirements:

- Adjacent uses are similar, so no buffer plantings are required.

### Trash enclosures, Electrical and Mechanical equipment, Utility Screening:

- All of the above shall be screened from streets or adjacent property with dense evergreen vegetation, a decorative opaque fence or wall complementing the architectural details and materials of the building, or architectural features of the building, or a combination.

### 69 TOTAL LARGE/MEDIUM TREES REQUIRED FOR PROJECT

### 33 TOTAL SMALL TREES REQUIRED FOR PROJECT

### 141 TOTAL SHRUBS REQUIRED FOR PROJECT

# MEIER LANDSCAPE ARCHITECTURE

15245 Metcalf Ave.  
Overland Park, KS 66223  
913.787.2817



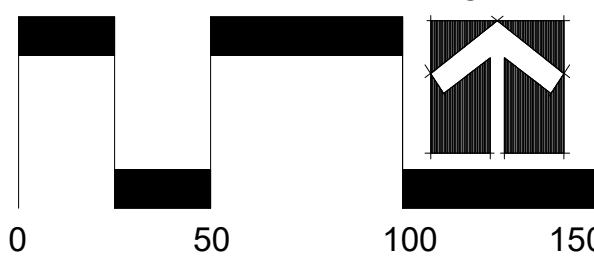
### CLIENT

New Life Church  
188th and Gardner Rd.  
Gardner, KS

### PROJECT

New Life Church  
188th and Gardner Rd.  
Gardner, KS

SCALE: 1"=50'



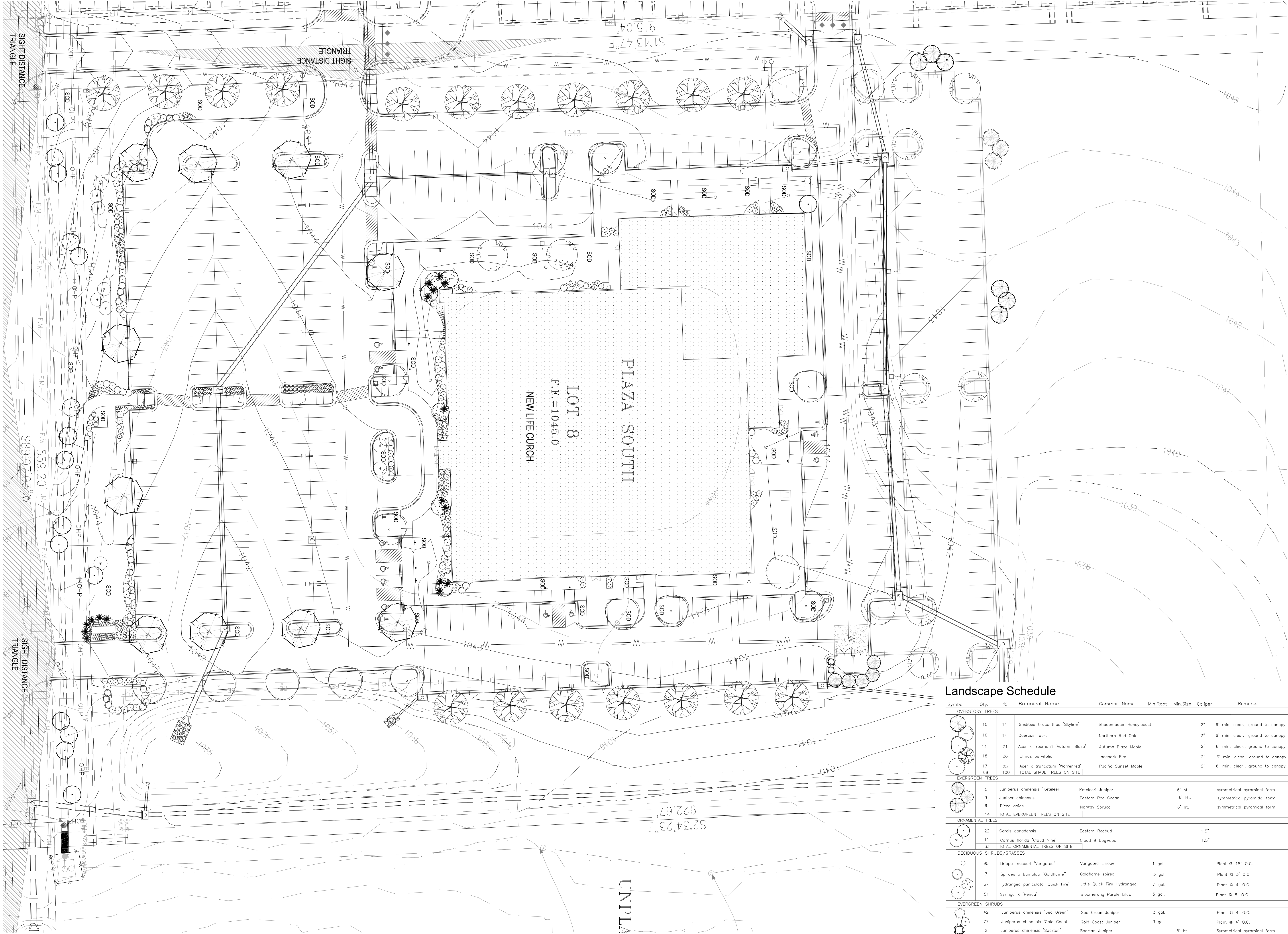
Date: 2.14.2020

Project #: 585

Landscape Plan

# L1




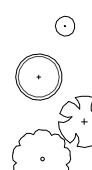




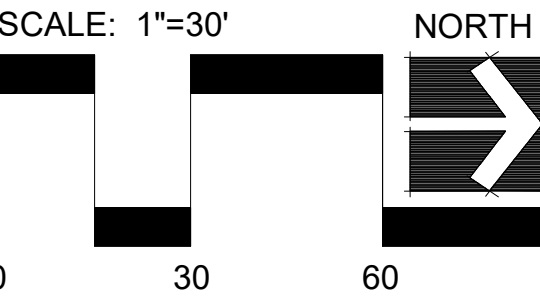


**CLIENT**  
New Life Church  
188th and Gardner Rd.  
Gardner, KS

**PROJECT**  
New Life Church  
188th and Gardner Rd.  
Gardner, KS

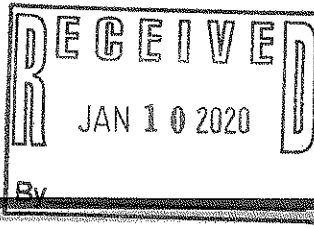
**Landscape Schedule**

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Date: 2.14.2020  
Project #: 585  
Landscape Plan

**L1**



Business & Economic Development  
Planning Division  
120 E. Main St. Gardner, KS 66030  
P: 913.856.0913 | F: 913.856.4562  
[www.gardnerkansas.gov](http://www.gardnerkansas.gov)

**SITE PLAN AND DESIGN REVIEW  
FINAL DEVELOPMENT PLAN  
APPLICATION**

Pre-App Date \_\_\_\_\_  
Fee \$300 pd.  
File No. FDP-20-01

**OWNER INFORMATION**

Name(s) New Life Community Church, Inc.  
Contact Rick George  
Address 17935 Moonlight Road  
City Gardner State KS Zip 66030  
Phone 913-856-5683 Email rick@newlifegardner.com

**APPLICANT/AGENT INFORMATION**

Name(s) Phelps Engineering, Inc.  
Contact Doug Ubben, Jr.  
Address 1270 N. Winchester  
City Olathe State KS Zip 66061  
Phone 913-393-1155 Email dougubben@phelpsengineering.com

**SITE INFORMATION**

Property Address/Location: 188th and Gardner Road  
Legal Description (Attach If Necessary) See Attached.  
Number of Existing Lots 1 Number of Proposed Lots 1  
Total Site Area 11.65 Acres Present Zoning ACP-2  
Proposed Use Church Present Land Use Agriculture  
Proposed Street Design Type(s) & Class N/A  
Proposed Type(s) Open & Civic Space Shelter's and benches along 188th Street  
Proposed Frontage Type(s) Terrace  
Proposed Building Types(s) Prominent Civic

**SIGNATURE**

I/We, the undersigned am/are the (owner(s)) (duly authorized agent), (Circle One) of the aforementioned property. By execution of my/our signature, I/we do hereby officially apply for a site plan and design review or final development plan as indicated above.

Signature(s): [Signature] Date 1/10/20  
Date \_\_\_\_\_

## SITE PLAN AND DESIGN REVIEW, AND FINAL DEVELOPMENT PLAN APPLICATION CHECKLIST

### APPLICATION SUBMITTAL REQUIREMENTS

- | Yes                                 | No                                  |   |
|-------------------------------------|-------------------------------------|---|
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 1. <b>Complete application packet</b>   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 2. <b>Application fee</b>   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 3. <b>10 complete sets of full sized plans printed including color elevations, folded</b>   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 4. <b>Digital copies (PDF) of the completed application, plans, and legal description</b>   |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | 5. <b>Copy of all covenants and restrictions applicable to the development, if applicable.</b>  |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | 6. <b>Sign posting affidavit.</b>   |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | 7. <b>Evidence of the establishment of the agency for the ownership and maintenance of any common open space and all assurances of the financial and administrative ability of such agency or document dedicating the facilities to the city.</b> |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | 8. <b>Deeds of dedication for all rights-of-way or easements</b> required as a result of preliminary development plan approval if conveyance thereof is not to be made by plat or by the filing of the final development plan.                    |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 9. <b>Evidence of satisfaction of any conditions of the preliminary development plan approval</b> which were conditions precedent to consideration of the final development plan.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 10. <b>Final Stormwater Management Plan</b> (2 printed and 1 digital copy) Memo   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 11. <b>Final Traffic Impact Study (TIS)</b> as required by the Access Management Code. (2 printed and 1 digital copy) Memo  |

### PLAN REQUIREMENTS

- |                                     |                          |   |
|-------------------------------------|--------------------------|---|
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 1. <b>Vicinity map</b> to scale with north point showing railroads, major streams or rivers, and public streets in the vicinity of the site. (Suggested scale of 1" = 1000'.)   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 2. <b>Boundary lines of the subdivision</b> shall be enclosed with one continuous bold line, showing approximate dimensions (bearings and distances).   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 3. <b>Scale, legend, and north arrow</b> clearly shown, with orientation at top or left as north (not less than 1"=100' scale).   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 4. <b>Dates of plan preparation</b> and/or plan revisions.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 5. <b>Ownership, zoning, and land use of the site and surrounding properties</b> within 200 feet; both existing and proposed.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 6. <b>Names, addresses, and phone numbers of all companies, firms, or individuals involved</b> in the preparation of the plan (i.e. developer, property owner, architect, landscape architect, planner, engineer, surveyor, etc.) |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 7. <b>Setback lines:</b> building and parking with dimensions in feet.  |

- | Yes                                 | No                                  |   |
|-------------------------------------|-------------------------------------|---|
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 8. <b>Setback lines:</b> building and parking with dimensions in feet.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 9. <b>Lots and tracts identified clearly</b> , with blocks numbered or lettered boldly and clearly in the center of the block, and lot dimensions with bearings and distances, and area in square feet and acres.   |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | 10. <b>Note on the plan indicating intended ownership, purpose, and maintenance responsibilities</b> for any parcels labeled as tracts.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 11. <b>Setback lines:</b> building and parking with dimensions in feet.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 12. <b>Location of existing open space, alleys, parks, streams, ponds, vegetation</b> or other similar features within plan area, and whether they are to be retained or removed.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 13. <b>Existing utilities</b> , including sanitary sewer, force main, water main, gas mains, culverts and storm sewer pipe, street lights, electric conduits, and invert elevations of sewers at points of proposed connection.   |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | 14. <b>Proposed street network</b> , including right-of-way, bearings, tangents, dimensions, and horizontal and vertical curvature data along the centerline of each street.  |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | 15. <b>All public streets within the plan conform</b> to the applicable minimum design standards set forth in the Land Development Code and Technical Specifications. Design of crosswalks, on street parking, shoulder, pavement and lane dimensions.  |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | 16. <b>Intersection site distance analysis.</b>   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 17. <b>Driveways, parking lots and stalls, aisles, and loading</b> and service areas and docks and dimensions.  |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | 18. <b>Median breaks and turning lanes</b> , including sizes and radii; both existing and proposed.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 19. <b>Vehicle maneuvering/turning templates</b> reflecting the site can accommodate a minimum SU-30 class vehicles (for emergency access to all areas of the site), and the appropriate site-design vehicle for any other special areas of the site (such as delivery or dock areas, etc.), as necessary.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 20. <b>Existing and proposed sidewalks</b> and/or trail locations including proposed widths.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 21. <b>Proposed utilities</b> , including approximate location of sanitary sewer, water main, and street lights.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 22. <b>Existing and proposed easements</b> with dimensions. Existing easements shall be labeled with book and page number.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 23. <b>Any area within a federally designated floodplain.</b> Location, stations, and elevations of the 100-year floodplain within the plan area and 100-year elevations at rear lot corners adjacent to FEMA and Shaded Zone X floodplains. The source of the floodplain information shall be clearly labeled (example: FIRM, Map #20091C0041D, September 27, 1991). |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | 24. <b>Stream corridor boundary</b> and dimensions.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | 25. <b>Phasing Plan</b> , if applicable.  |



- | Yes                                 | No                       |  |
|-------------------------------------|--------------------------|--|
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 26. <b>Planned amenities</b> , such as fountains, art, outdoor seating, waste receptacles, etc.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 27. <b>Any buildings within the plan area</b> which are existing or proposed, with status indicated including dimensions (i.e. to remain, remodel, new, to be demolished as part of Phase 2, etc.).  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 28. <b>Distances between all buildings</b> , between buildings and property lines, and between parking areas and property lines.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 29. <b>Existing Topography and Proposed Grading</b> of the area contained in the plan area and within 20 feet of the boundary shown by 2-foot contour intervals. Contour lines shall be legible but not overpowering.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 30. <b>Building elevations</b> depicting the architectural style, size, exterior construction materials, and colors for each type of building proposed, and dimensions. If an architectural theme is planned, elaboration on the intent and extent of the scheme and details shall be provided. Elevations shall provide sufficient information to determine relationships between various elements, building height, proportion, bedroom counts, approximate square footage, etc. Rooftop and ground-mounted mechanical equipment shall be shown on elevations. |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 31. <b>Screen walls, fences, trash enclosures, and mail kiosks</b> (existing and proposed), including location, height, and materials.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 32. <b>Table indicating lots, land areas, buildings, number of stories, building coverage, and all other quantities relative</b> to the submitted plan that are required to determine compliance with City codes. For commercial buildings, indicate service floor areas and number of tenant spaces, if applicable. For residential buildings, indicate dwelling units; if multiple building types, provide an additional table indicating dwelling units by building type.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 33. <b>Table indicating required and proposed parking spaces.</b>  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 34. <b>Landscaping plan and table</b> indicating all proposed landscaping, noting common and botanical names, numbers, and planting sizes. Note all other areas to be sodded.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 35. <b>All exterior sign locations.</b> Include elevations and details.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 36. <b>All outside lighting facilities:</b> Location, height, wattage, and type including shielding, for buildings, parking lots and outdoor storage areas.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 37. <b>Outdoor storage areas</b> , including location, dimensions and design.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 38. <b>Planned amenities</b> , such as fountains, art, outdoor seating, waste receptacles, etc.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 39. <b>Preliminary design and location of all proposed storm drainage</b> conveyance, detention and treatment facilities and locations of existing drainage facilities.  |

I hereby submit all information required for a site plan and design review, or final development plan application review. I understand that failure to provide the required information may result in a postponement of my request for review until all information has been submitted.

Signature of Applicant

Date

**PLANNING COMMISSION STAFF REPORT**  
**MEETING DATE: FEBRUARY 25, 2020**  
**PREPARED BY: ROBERT CASE, PLANNER**

**NEW BUSINESS ITEM NO. 3A**

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**PROJECT NUMBER / TITLE: PP-20-02: Preliminary plat for Frontier Commerce Park 1<sup>st</sup> Plat**

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**PROCESS INFORMATION**

**Type of Request:** Preliminary Plat  
**Date Received:** January 10, 2020

**APPLICATION INFORMATION**

**Applicant:** Jim Long P.E., Anderson Engineering  
**Owner:** Frontier Community Credit Union, Michael Augustine  
**Parcel ID:** CF221424-4032 and CF221424-4028  
**Location:** On the West side of Moonlight Road approximately 240' south of Madison Street

**REQUESTED ACTION**

The applicant requests approval of a preliminary plat for a 2 lot commercial project containing 3.24 acres.

**EXISTING ZONING AND LAND USE**

The subject properties are currently vacant and unplatted parcels of land zoned C-2 (General Business) District.

**SURROUNDING ZONING AND LAND USE**

<b>Zoning</b>	<b>Use(s)</b>
<b>North of subject property</b>	
R-1 (Single Family Residential) District	Single-Family Housing
C-2 (General Business) District	Convenience Store
<b>East of subject property</b>	
CP-2 (Planned General Business) District	Multi-tenant Commercial Center
<b>South of subject property</b>	
C-3 (Commercial) District	Equipment Rental Store
<b>West of subject property</b>	
RP-3 (Planned Garden Apartment) District	Single and Multi-Family Housing

## **EXISTING CONDITIONS**

These two parcels are vacant and surrounded by both residential and commercial uses. The residential to the north and west are single-family and apartments. To the south and east are both commercial uses.



## **BACKGROUND / HISTORY**

The property was annexed in 1970 under Ordinance Number 1015. The northeast side of the original parcel was platted in 2014 for the Casey's General Store.

## **CONSISTENCY WITH COMPREHENSIVE PLAN**

The existing land use is consistent with the Land Use Plan map of the *Gardner Comprehensive Plan* (specifically the *Gardner Main Street Corridor Plan*, adopted as part of the Comprehensive Plan) that designates this property for Community Mixed Use described as follows: *Community Mixed Use areas are intended to provide retail and professional services for the everyday needs of the people residing or working in the community. Uses should be limited to those that meet the needs of residents such as civic uses, grocery and retail stores, restaurants, hotels, professional services, and entertainment venues, as well as office and medical commercial uses. Community mixed uses can be located in stand-alone structures, or as part of a small shopping center with multiple uses, and may include complementary residential development that provides opportunities for housing near jobs and essential goods and services. These areas are developed with a focus on pedestrian safety and connectivity and support of alternative transportation options. Vehicular circulation and connectivity is accommodated in a less visually prominent pattern, minimizing the number of access drives and screening parking areas from the public space.*



## **STAFF ANALYSIS**

### **PRELIMINARY PLAT**

#### **17.03.020 (D1) Review Criteria:**

- a. The application is in accordance with the Comprehensive Plan and in particular the physical patterns, arrangement of streets, blocks, lots and open spaces, and public realm investments that reflect the principles and concepts of the plan.

**Staff Comment:** *The project is for the platting of two parcels to create 2 buildable lots. The newly created lots will gain access onto both Madison Street and Moonlight Road through existing curb cuts. Staff is requiring a cross-access easement to the commercial development just south of this subdivision to further limit access onto Moonlight Road while providing an efficient circulation system throughout the west side of Moonlight Road for these commercialized areas. Staff is also requiring a pedestrian easement to be dedicated along the south lot lines of both Lot 1 and 2 for construction of a 5' sidewalk. This will help to provide connectivity through the long neighborhood block to the west. Both of these requirements are consistent with the Community Commercial land use areas which states that commercial should be well-connected to the surrounding community to ensure that commercial areas integrate pedestrian and bicycle connections to adjacent neighborhoods and encourage cross access between adjacent commercial uses to reduce traffic on City roads.*

- b. Compliance with the requirements of this Land Development Code, and in particular the blocks and lots proposed are capable of meeting all development and site design standards under the existing or proposed zoning.

**Staff Comment:** *This development is in compliance with the Land Development Code and is capable of meeting all the development and sight design standards under the existing zoning.*

- c. Any phasing proposed in the application is clearly indicated and demonstrates a logical and coordinated approach to development, including coordination with existing and potential development on adjacent property.

**Staff Comment:** *There is no phasing proposed for the plat.*

- d. Any impacts identified by specific studies or technical reports, including a preliminary review of storm water, are mitigated with generally accepted and sound planning, engineering, and urban design solutions that reflect long-term solutions and sound fiscal investments.

**Staff Comment:** *The Stormwater Management Plan and Traffic Impact Study have not been approved by Public Works Department. Both reports will need to be submitted and approved by Public Works prior to the release of the plat for recording.*

- e. The application does not deter any existing or future development on adjacent property from meeting the goals and policies of the Comprehensive Plan.

**Staff Comment:** *The property directly adjacent is developed and with ample cross-access and pedestrian easements, this development will not deter any existing or future development on adjacent property.*

- f. The design does not impede the construction of anticipated or planned future public infrastructure within the area.

**Staff Comment:** *The design does not impede construction of future public infrastructure as none are anticipated in this area in the near future.*

- g. The recommendations of professional staff, or any other public entity asked to officially review the plat.

**Staff Comment:** *The preliminary plat meets all the applicable standards of the Land Development Code with exception to the approval of the stormwater and traffic studies. Staff recommends approval of the preliminary plat for Frontier Commerce Park – 1<sup>st</sup> Plat with conditions to obtain approval of those documents.*

*This application is within the joint review for the impact area of the New Century AirCenter and therefore will go before the Airport Commission on March 25<sup>th</sup> and the Board of County Commissioners in March.*

### **STAFF ANALYSIS – INFRASTRUCTURE / OTHER**

#### **WATER, SANITARY SEWER, STORMWATER, ELECTRIC, GAS –**

Existing utilities are located either within or adjacent to the site.

#### **ROADWAY NETWORK, VEHICULAR ACCESS –**

There are no new points of access being proposed for this site. Access off of Moonlight Road will be through an existing drive that will provide access throughout the development. An existing access point onto Madison Street will provide secondary ingress/egress throughout the site as well. A cross-access easement is being provided to connect the property to the south in order to limit the amount of curb cuts onto Moonlight Road and provide a more efficient means of movement of vehicular traffic along the west side of the street.

#### **SIDEWALKS –**

An existing 10' trail is located along the west side of Moonlight Road along with a 5' sidewalk extending along the south side of Madison Street. As mentioned before, another 5' sidewalk is being proposed along the entire south property line to provide for access to the residential development west.

### **ATTACHMENTS**

- I. Plat document
- II. Application

### **ACTIONS**

Per Section 17.03.010 (G) of the *Gardner Land Development Code*, a review body may take the following actions (or recommend the following actions):

1. Approve the application.
2. Approve the application with conditions or modifications to lessen or mitigate a potential impact from the proposed application.
3. Deny the application.
4. Continue the application to allow further analysis. The continued application shall not be more than 60 days from the original review without consent of the applicant. No application shall be continued more than once by each review body without consent of the applicant.

### **EFFECT OF DECISION**

Preliminary Plat – The approval of the preliminary plat does not constitute an acceptance of the subdivision, but authorizes preparation of the final plat. If the Planning Commission tables a plat, the applicant shall have 60 days to submit information sufficient for approval or the application shall be deemed denied. The applicant may request that a denied preliminary plat be submitted to the Governing Body and the Planning Commission shall submit all information to the Governing Body, which can make a determination consistent with these regulations. The approval of the preliminary plat shall be effective for 18 months, except that any approval of a final plat for any phase specifically indicated on a preliminary plat shall renew the 18-month period. The Planning Commission may grant an extension of this period for up to one year, if the applicant demonstrates substantial progress towards the design and engineering requirements necessary to submit a final plat.

### **RECOMMENDATION**

Staff recommends approval of the Preliminary Plat subject to the following condition:

1. Approval of the Traffic Impact Study and Stormwater Management Plan by the City of Gardner Public Works Department prior to the release of the plat for recording.
2. Review and approval at the Johnson County Airport Commission and Board of County Commission shall be completed prior to the issuance of a building permit.

### **Recommended Motion:**

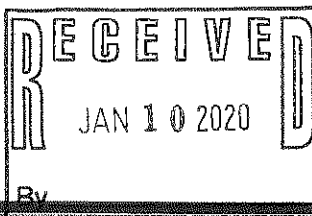
After review of Application PP-20-02, a preliminary plat for Frontier Commerce Park – 1<sup>st</sup> Plat, located on the west side of Moonlight Road, approximately 240' south of Madison Street, (Tax Ids CF221424-4032 and CF221424-4028) and preliminary plat dated February 25, 2020, and staff report dated February 18, 2020, the Planning Commission approves the application as proposed, provided the following condition is met:

1. Approval of the Traffic Impact Study and Stormwater Management Plan by the City of Gardner Public Works Department prior to the release of the plat for recording.
2. Review and approval at the Johnson County Airport Commission and Board of County Commission shall be completed prior to the issuance of a building permit.



SHEET NUMBER **1** OF 1





Business & Economic Development  
Planning Division  
120 E. Main St. Gardner, KS 66030  
P: 913.856.0913 | F: 913.856.4562  
[www.gardnerkansas.gov](http://www.gardnerkansas.gov)

## PRELIMINARY PLAT APPLICATION

Pre-App Date	_____
Fee	\$ <u>264</u>
File No.	<u>PP-20-02</u>

### OWNER INFORMATION

Name(s) Frontier Community Credit Union  
Contact Michael Augustine  
Address 690 Eisenhower Road  
City Leavenworth State KS Zip 66048  
Phone 913.651.6575 Email maugustine@frontierccu.org

### APPLICANT/AGENT INFORMATION

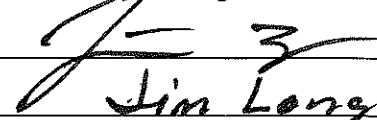
Name(s) Anderson Engineering  
Contact Jim Long, P.E.  
Address 941 W 141st Terrace, Suite A  
City Kansas City State MO Zip 64145  
Phone 816.777.0400 Email jlong@andersonengineeringinc.com

### SITE INFORMATION

Property Address/Location Southwest Corner of E. Madison St. and Moonlight Rd. (south and west of Casey's)  
Legal Description (Attach If Necessary) See Attached  
Number of Existing Lots 2 Tracts Number of Proposed Lots 2 Lots  
Total Site Area 3.24 ac. Present Zoning C-2  
Number of Existing Structures 0 Present Land Use Vacant  
Proposed Street Design Type(s) & Class N/A  
Proposed Type(s) Open & Civic Space \_\_\_\_\_  
Proposed Frontage Type(s) \_\_\_\_\_  
Proposed Building Types(s) \_\_\_\_\_

### SIGNATURE

I/We, the undersigned am/are the (owner(s)), (fully authorized agent), (Circle One) of the aforementioned property. By execution of my/our signature, I/we do hereby officially apply for preliminary plat as indicated above.

Signature(s):  Date 1/10/2020  
Date \_\_\_\_\_

## PRELIMINARY PLAT APPLICATION CHECKLIST

### APPLICATION SUBMITTAL REQUIREMENTS

- |     | Yes                                 | No                       |  |
|-----|-------------------------------------|--------------------------|--|
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 1. <b>Complete application packet</b>  |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 2. <b>Application fee</b>  |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 3. <b>10 complete sets of full sized plans printed and folded</b>  |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 4. <b>Digital copies (PDF) of the completed application, plans, and legal description</b>  |
|     | <input type="checkbox"/>            | <input type="checkbox"/> | 5. <b>1 copy of existing covenants and restrictions</b> applicable to the development, if any (reference book and page).   |
| N/A | <input type="checkbox"/>            | <input type="checkbox"/> | 6. <b>Letter of intent</b> as to whether a Homeowners Association will be created and if any covenants and restrictions are proposed by the subdivider.  |
| N/A | <input type="checkbox"/>            | <input type="checkbox"/> | 7. <b>Preliminary Floodplain modeling</b> using HEC-RAS model provided by the City if encroachment is proposed within a FEMA or Shaded Zone X 100-year floodplain. (Contact City Engineer to obtain model and requirements). |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 8. <b>Preliminary Stormwater Management Plan</b> (2 printed and 1 digital copy)  |
| N/A | <input type="checkbox"/>            | <input type="checkbox"/> | 9. <b>Preliminary Traffic Assessment</b> See Access Management Code. (2 printed and 1 digital copy)  |

### PRELIMINARY PLAT REQUIREMENTS

- |     |                                     |                          |  |
|-----|-------------------------------------|--------------------------|--|
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 1. <b>Name of subdivision</b> (unique and numerically consistent and the words "PRELIMINARY PLAT," prominently displayed as the title.)  |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 2. <b>Names, addresses, and phone numbers</b> of all companies, firms, or individuals involved in the preparation of the plat (i.e. property owner, engineer, surveyor, etc.).   |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 3. <b>Date of preparation</b> of preliminary plat and/or revisions.  |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 4. <b>Vicinity map</b> (drawn at a scale of 1"=2,000', locating the proposed subdivision in relation to the section of land, including township and range, section street names, and a north arrow.)                           |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 5. <b>A legal boundary description</b> with angular bearings and linear distances, referenced to section or quarter-section corners, Point of Commencing and/or Point of Beginning, and the overall area of the plat in acres. |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 6. <b>Location of monuments</b> , shown in reference to existing official monuments or the nearest established ¼ section corner, including the bearings and distances to such reference points or monuments.                   |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 7. <b>Boundary lines</b> of the subdivision shall be enclosed with one continuous bold line, showing approximate dimensions (bearings and distances).  |
| N/A | <input type="checkbox"/>            | <input type="checkbox"/> | 8. <b>All public streets</b> within the plat conform to the applicable minimum design standards set forth in the Land Development Code and Technical Specifications.   |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 9. <b>Building setback lines</b> along public and private streets with dimensions in feet.   |

- |     | Yes                                 | No                       |   |
|-----|-------------------------------------|--------------------------|---|
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 10. <b>Platted and unplatted land</b> adjacent to the plat boundary. Include identification of adjacent platted subdivisions and unplatted tracts with external bearings and distances of adjacent plats and property owners for a distance of not less than 400 feet. Include original plat names if replatted. Exterior dimensions shall coincide with adjoining plats unless differences are noted |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 11. <b>Lots and tracts</b> identified clearly, with blocks numbered or lettered boldly and clearly in the center of the block, and lot dimensions with bearings and distances, and area in square feet.   |
| N/A | <input type="checkbox"/>            | <input type="checkbox"/> | 12. <b>Note on plat</b> indicating intended ownership, purpose, and maintenance responsibilities for any parcels labeled as tracts.   |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 13. <b>Existing streets, driveways, trails, and sidewalks</b> which abut, touch upon or extend through the subdivision and/or streets located within 400 feet of the plat. The description shall include types and widths of existing surfaces, right-of-way widths, and dimensions of any bridges and culverts, access points and signals.   |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 14. <b>Location of existing open space, alleys, parks, streams, ponds, vegetation</b> , or other similar features within plat, and whether they are to be retained or removed.  |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 15. <b>Location of existing buildings</b> and structures within 200 feet of the plat.   |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 16. <b>Existing utilities</b> , including sanitary sewer, force main, water main, gas mains, culverts and storm sewer pipe, street lights, electric conduits, and invert elevations of sewers at points of proposed connection.   |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 17. <b>Topography</b> of the area contained in the plat and within 20 feet of the plat boundary shown by 2-foot contour intervals and proposed preliminary grading. Contour lines shall be legible but not overpowering.  |
| N/A | <input type="checkbox"/>            | <input type="checkbox"/> | 18. <b>Proposed street network</b> , including right-of-way, bearings, tangents, and horizontal and vertical curvature data (use of flow direction arrows and percent of grade is permitted at preliminary for vertical curve data, unless otherwise specified/required) along the centerline of each street.   |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 19. <b>Proposed sidewalks</b> and/or trail locations including proposed widths.   |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 20. <b>Proposed utilities</b> , including approximate location of sanitary sewer, water main, street lights, storm sewer, detention and treatment facilities.   |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 21. <b>Existing and proposed easements</b> with dimensions. Existing easements shall be labeled with book and page number.  |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 22. <b>Any area within a federally designated floodplain</b> . Location, stations, and elevations of the 100-year floodplain within the plat and 100-year elevations at rear lot corners adjacent to FEMA and Shaded Zone X floodplains. The source of the floodplain information shall be clearly labeled (example: FIRM, Map #20091C0041D, September 27, 1991).                                     |
| N/A | <input type="checkbox"/>            | <input type="checkbox"/> | 23. <b>Stream corridor boundary</b> and dimensions.   |
| N/A | <input type="checkbox"/>            | <input type="checkbox"/> | 24. <b>Intersection site distance analysis</b> .  |
| N/A | <input type="checkbox"/>            | <input type="checkbox"/> | 25. <b>Copies of all pertinent exception documents</b> , or a copy of a current American Land Title Association (ALTA) survey, or both.   |

**Written explanations** for any items not checked or checked "No" (attach additional sheets, if necessary):

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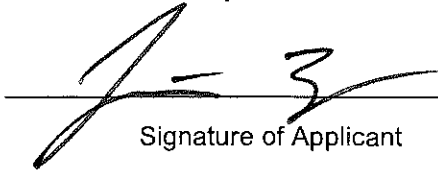
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I hereby submit all information required for preliminary plat review. I understand that failure to provide the required information may result in a postponement of my request for review until all information has been submitted. By signing this application, I acknowledge that all public streets and public infrastructure within the plat shall conform to the applicable minimum design standards set forth in the Gardner Municipal Code and the Technical Specifications.

  
\_\_\_\_\_  
Signature of Applicant  
\_\_\_\_\_  
Date



**PLANNING COMMISSION STAFF REPORT**  
**MEETING DATE: FEBRUARY 25, 2020**  
**PREPARED BY: ROBERT CASE, PLANNER**

**NEW BUSINESS ITEM No. 3B**

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**PROJECT NUMBER / TITLE: FP-20-01: Final Plat for Frontier Commerce Park, 1<sup>st</sup> Plat**

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**PROCESS INFORMATION**

**Type of Request:** Final Plat  
**Date Received:** January 10, 2020

**APPLICATION INFORMATION**

**Applicant:** Jim Long P.E., Anderson Engineering  
**Owner:** Frontier Community Credit Union, Michael Augustine  
**Parcel ID:** CF221424-4032 and CF221424-4028  
**Location:** On the West side of Moonlight Road approximately 240' south of Madison Street

**REQUESTED ACTION**

The applicant requests approval of a final plat for a 2 lot commercial project containing 3.24 acres.

**EXISTING ZONING AND LAND USE**

The subject properties are currently vacant and unplatted parcels of land zoned C-2 (General Business) District.

**SURROUNDING ZONING AND LAND USE**

<b>Zoning</b>	<b>Use(s)</b>
<b>North of subject property</b>	
R-1 (Single Family Residential) District	Single Family Housing
C-2 (General Business)	Convenience Store
<b>East of subject property</b>	
CP-2 (Planned General Business) District	Multi-tenant Commercial Center
<b>South of subject property</b>	
C-3 (Commercial) District	Equipment Rental Store
<b>West of subject property</b>	
RP-3 (Planned Garden Apartment) District	Single and Multi-Family Housing

## **EXISTING CONDITIONS**

These two parcels are vacant and surrounded by both residential and commercial uses. The residential to the north and west are single-family and apartments. To the south and east are both commercial stores.



## **BACKGROUND / HISTORY**

The property was annexed in 1970 under ordinance number 1015. The northeast side of the original parcel was platted in 2014 for the Casey's General Store.

## **CONSISTENCY WITH COMPREHENSIVE PLAN**

The existing land use is consistent with the Land Use Plan map of the *Gardner Comprehensive Plan* (specifically the *Gardner Main Street Corridor Plan*, adopted as part of the Comprehensive Plan) that designates this property for Community Mixed Use described as follows: *Community Mixed Use areas are intended to provide retail and professional services for the everyday needs of the people residing or working in the community. Uses should be limited to those that meet the needs of residents such as civic uses, grocery and retail stores, restaurants, hotels, professional services, and entertainment venues, as well as office and medical commercial uses. Community mixed uses can be located in stand-alone structures, or as part of a small shopping center with multiple uses, and may include complementary residential development that provides opportunities for housing near jobs and essential goods and services. These areas are developed with a focus on pedestrian safety and connectivity and support of alternative transportation options. Vehicular circulation and connectivity is accommodated in a less visually prominent pattern, minimizing the number of access drives and screening parking areas from the public space.*

## **STAFF ANALYSIS**

### **FINAL PLAT**

#### **17.03.020 (E1) Review Criteria:**

- a. The layout and design of the final plat is in substantial compliance with the approved preliminary plat considering the number of lots or parcels; the block layout, street designs

and access; the open space systems and civic design elements; the infrastructure systems; or other elements of coordinated developments.

**Staff Comment:** *The final plat is in substantial compliance with the approved preliminary plat.*

- b. The construction plans for any utilities, infrastructure or public facilities shall have been found to meet all technical specifications, or final plat approval shall be conditioned on such plans meeting all technical specifications, before the recording of the final plat.

**Staff Comment:** *Public improvement plans shall be submitted and approved prior to the release of the final plat for recording at the County (this is a recommended condition of approval).*

- c. The phasing and timing of public improvements ensures construction and performance guarantees.

**Staff Comment:** *Public improvement plans, the Traffic Impact Study and Stormwater Management Plan shall be submitted and approved prior to the release of the final plat for recording and issuance of building permits to ensure all improvements are in place.*

- d. Any deviations in the final plat from the preliminary plat brings the application in further compliance with the Comprehensive Plan and the purposes and intent of this Code.

**Staff Comment:** *There are no deviations being requested on the final plat.*

- e. The recommendations of professional staff, or any other public entity asked to officially review the plat.

**Staff Comment:** *Staff recommends approval of the project with the conditions outlined below. The final plat meets all the applicable standards of the Land Development Code with exception to the approval of the stormwater and traffic studies. Staff recommends approval of the final plat for Frontier Commerce Park – 1<sup>st</sup> Plat with conditions to obtain approval of these documents.*

*This application is within the joint review for the impact area of the New Century AirCenter and therefore will go before the Airport Commission on March 25<sup>th</sup> and the Board of County Commissioners in March.*

### **EXCISE TAX**

Excise tax is levied with the act of platting the portion of the property in the city. Any of the subject property that has never been a part of a final plat before is therefore subject to paying the excise tax. This tax is based on the square footage of the plat property, excluding any arterial type right-of-way dedication for streets and parkland dedication.

The current tax rate is \$0.20 per square foot of land area platted. This plat includes 3.24 acres (141,134.4 sq.ft.) that has not paid excise tax before. Therefore the excise tax shall total **\$28,226.88**. This tax shall be paid prior to the Mayor signing an approved recordable plat.

### **STAFF ANALYSIS – INFRASTRUCTURE / OTHER**

#### **WATER, SANITARY SEWER, STORMWATER, ELECTRIC, GAS –**

Existing utilities are located either within and adjacent to the site.

## **ROADWAY NETWORK, VEHICULAR ACCESS –**

There are no new points of access being proposed for this site. Access off of Moonlight Road will be through an existing drive that will provide access throughout the development. An existing access point onto Madison Street will provide secondary ingress/egress throughout the site as well. A cross-access easement is being provided to connect the property to the south to limit the amount of curb cuts onto Moonlight and provide a more efficient means of movement of vehicular traffic along the west side of the street.

## **SIDEWALKS –**

An existing 10' trail is located along the west side of Moonlight Road along with a 5' sidewalk extending along the south side of Madison Street. As mentioned before, another 5' sidewalk is being proposed along the entire south property line to provide for access to the residential development west.

## **ATTACHMENTS**

- I. Final Plat
- II. Application

## **ACTIONS**

Per Section 17.03.010 (G) of the *Gardner Land Development Code*, a review body may take the following actions (or recommend the following actions):

1. Approve the application.
2. Approve the application with conditions or modifications to lessen or mitigate a potential impact from the proposed application.
3. Deny the application.
4. Continue the application to allow further analysis. The continued application shall not be more than 60 days from the original review without consent of the applicant. No application shall be continued more than once by each review body without consent of the applicant.

## **EFFECT OF DECISION**

Final Plat – If the Planning Commission approves or conditionally approves the final plat, the plat shall be forwarded to the Governing Body with a recommendation that they accept dedication of land for public purposes such as easements, rights-of-way and public facilities. The approval of the final plat; acceptance of the dedication of land for public purposes; finding that the construction plans for any utilities, infrastructure or public facilities meet all City technical specifications; and payment of the excise tax if applicable, authorizes the filing of the plat with the Johnson County Records and Tax Administration. Any approval with conditions or exceptions to the rules shall be clearly stated on the plat. Any plat not recorded within two years from the date of acceptance of land by the Governing Body shall be null and void. Upon approval of the final plat, dedications, and construction plans and recording of the plat, the applicant may proceed with the construction of required improvements. No building permit shall be authorized until the completion, inspection and acceptance of all required improvements.

## **RECOMMENDATION**

Staff recommends approval of FP-20-01, a final plat for Frontier Commerce Park – 1<sup>st</sup> Plat subject to the following conditions:

1. Preliminary plat PP-20-02 shall be approved prior to the release of the final plat FP-20-01 for recording.
2. Approval of the Traffic Impact Study by the City of Gardner Public Works Department.
3. Approval of the Stormwater Management Plan by the City of Gardner Public Works Department.
4. Provide a maintenance agreement for the shared access easements prior to the release of the plat for recording.
5. Payment of excise tax to the City.
6. Approval of the application by Johnson County Airport Commission and the Johnson County Board of County Commissioners is completed prior to the release of the plat for recording.

### **Recommended Motion:**

After review of Application FP-20-02, a preliminary plat for Frontier Commerce Park – 1<sup>st</sup> Plat, located on the west side of Moonlight Road approximately 240' south of Madison Street (Tax Ids CF221424-4032 and CF221424-4028) and final plat dated February 25, 2020, and staff report dated February 18, 2020, the Planning Commission approves the application as proposed, provided the following conditions are met:

1. Preliminary plat PP-20-02 shall be approved prior to the release of the final plat FP-20-01 for recording.
2. Approval of the Traffic Impact Study by the City of Gardner Public Works Department.
3. Approval of the Stormwater Management Plan by the City of Gardner Public Works Department.
4. Provide a maintenance agreement for the shared access easements prior to the release of the plat for recording.
5. Payment of excise tax to the City.
6. Approval of the application by Johnson County Airport is completed prior to the release of the plat for recording.

and recommends the Governing Body accept dedication of right-of-way and easements.



**MADISON STREET**  
(80' R/W) & (Public Utility)  
Vol. 2878, Pg. 534



Closure Error Distance> 0.0010 Error Bearing> N 17°35'25" W  
Closure Precision> 1 in 1910604.2

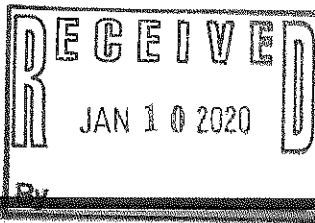
KANSAS CERTIFICATE OF AUTHORITY LS-17

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**SHEET NUMBER**

1 OF 1





Business & Economic Development  
Planning Division  
120 E. Main St. Gardner, KS 66030  
P: 913.856.0913 | F: 913.856.4562  
[www.gardnerkansas.gov](http://www.gardnerkansas.gov)

## FINAL PLAT APPLICATION

Pre-App Date	_____
Fee	\$ <u>264</u>
File No.	<u>FP-20-01</u>

### OWNER INFORMATION

Name(s) Frontier Community Credit Union  
Contact Michael Augustine  
Address 690 Eisenhower Road  
City Leavenworth State KS Zip 66048  
Phone 913.651.6575 Email maugustine@frontierccu.org

### APPLICANT/AGENT INFORMATION

Name(s) Anderson Engineering  
Contact Jim Long, P.E.  
Address 941 W 141st Terrace, Suite A  
City Kansas City State MO Zip 64145  
Phone 816.777.0400 Email jlong@andersonengineeringinc.com

### SITE INFORMATION

Property Address/Location Southwest Corner of E. Madison St. and Moonlight Rd. (south and west of Casey's)  
Legal Description (Attach If Necessary) See Attached  
Number of Existing Lots 2 Tracts Number of Proposed Lots 2  
Total Site Area 3.24 ac. Present Zoning C-2  
Number of Existing Structures 0 Present Land Use Vacant  
Proposed Street Design Type(s) & Class N/A  
Proposed Type(s) Open & Civic Space \_\_\_\_\_  
Proposed Frontage Type(s) \_\_\_\_\_  
Proposed Building Types(s) \_\_\_\_\_

### SIGNATURE

I/We, the undersigned am/are the (owner(s)), (duly authorized agent), (Circle One) of the aforementioned property. By execution of my/our signature, I/we do hereby officially apply for final plat as indicated above.

Signature(s):  Date 1/10/2020  
Date \_\_\_\_\_

## FINAL PLAT APPLICATION CHECKLIST

### APPLICATION SUBMITTAL REQUIREMENTS

- | Yes                                 | No                       |  |
|-------------------------------------|--------------------------|--|
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 1. <b>Complete application packet</b>  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 2. <b>Application fee</b>  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 3. <b>10 full sized plans printed and folded</b>   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 4. <b>Digital copies (PDF) of the completed application, plans, and legal description</b>  |
| N/A <input type="checkbox"/>        | <input type="checkbox"/> | 5. <b>1 copy of existing covenants and restrictions</b> applicable to the development, if any (reference book and page).   |
| N/A <input type="checkbox"/>        | <input type="checkbox"/> | 6. <b>Letter of intent</b> as to whether a Homeowners Association will be created and if any covenants and restrictions are proposed by the subdivider, if not submitted with the Preliminary Plat. Covenants and restrictions, as well as evidence of the establishment of the agency for the ownership and maintenance of any common space, shall be submitted to the City for review and approval prior to recording of the plat. |
| N/A <input type="checkbox"/>        | <input type="checkbox"/> | 7. <b>Final Floodplain modeling</b> using HEC-RAS model provided by the City if encroachment is proposed within a FEMA or Shaded Zone X 100-year floodplain. (Contact City Engineer to obtain model and requirements).   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 8. <b>Final Stormwater Management Plan</b> (2 printed and 1 digital copy)  |
| N/A <input type="checkbox"/>        | <input type="checkbox"/> | 9. <b>Final Traffic Impact Study (TIS)</b> as required by the Access Management Code. (2 printed and 1 digital copy)   |
| N/A <input type="checkbox"/>        | <input type="checkbox"/> | 10. <b>Development Agreement</b> , if required   |
| N/A <input type="checkbox"/>        | <input type="checkbox"/> | 11. <b>Street tree plan</b>  |

### FINAL PLAT DOCUMENT REQUIREMENTS

- |                                     |                          |  |
|-------------------------------------|--------------------------|--|
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 1. <b>Name of subdivision</b> (unique and numerically consistent and the words "FINAL PLAT," prominently displayed as the title.)  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 2. <b>Names, addresses, and phone numbers</b> of all companies, firms, or individuals involved in the preparation of the plat (i.e. property owner, engineer, surveyor, etc.).   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 3. <b>Date of preparation</b> and/or revisions.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 4. <b>Vicinity map</b> (drawn at a scale of 1"=2,000', locating the proposed subdivision in relation to the section of land, including township and range, section street names, and a north arrow.)                           |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 5. <b>A legal boundary description</b> with angular bearings and linear distances, referenced to section or quarter-section corners, Point of Commencing and/or Point of Beginning, and the overall area of the plat in acres. |

Yes No

- ☒ ☐ 6. **Provide the following sentence after the Legal Description** "The undersigned proprietors of the above described tract of land have caused the same to be subdivided in the accompanying plat, which subdivision and plat shall hereafter be known as "Plat Name".
- ☒ ☐ 7. **Location of monuments**, shown in reference to existing official monuments or the nearest established  $\frac{1}{4}$  section corner, including the bearings and distances to such reference points or monuments.
- ☒ ☐ 8. **Boundary lines** of the subdivision shall be enclosed with one continuous bold line, showing approximate dimensions (bearings and distances).
- X ☐ 9. **Accurate dimensions for all lines, angles, and curves**, used to describe boundaries, streets, easements and areas to be reserved for public use. Data for all curves shall include radius, arc length, chord length, and central angle.
- ☒ ☐ 10. **Platted and unplatted land** adjacent to the plat boundary. Include identification of adjacent platted subdivisions and unplatted tracts with external bearings and distances of adjacent plats and property owners for a distance of not less than 400 feet. Include original plat names if replatted. Exterior dimensions shall coincide with adjoining plats unless differences are noted
- ☒ ☐ 11. **Blocks, lots and tracts** identified clearly, with blocks numbered or lettered boldly and clearly in the center of the block, and lot dimensions with bearings and distances, and area in square feet.
- N/A ☐ ☐ 12. **Note on plat** indicating intended ownership, purpose, and maintenance responsibilities for any parcels labeled as tracts.
- ☒ ☐ 13. **Existing and proposed easements** with dimensions. Existing easements shall be labeled with book and page number.
- ☒ ☐ 14. **Any area within a federally designated floodplain**. Location, stations, and elevations of the 100-year floodplain within the plat and 100-year elevations at rear lot corners adjacent to FEMA and Shaded Zone X floodplains. The source of the floodplain information shall be clearly labeled (example: FIRM, Map #20091C0041D, September 27, 1991).
- N/A ☐ ☐ 15. **Stream corridor boundary** and dimensions.
- N/A ☐ ☐ 16. **Proposed street right-of-way with dimensions** which conform to the applicable minimum design standards set forth in the Land Development Code and Technical Specifications.
- ☐ ☐ 17. **Endorsement of the Planning Commission** as evidenced by the signature of its Chairperson. Endorsement line shall contain the printed name of the Chairperson and their title.
- ☐ ☐ 18. **Acceptance of Dedication by the Governing Body**, as indicated by the signature of the Mayor and attested by the City Clerk. The Endorsement Line shall contain the printed name and title of the person signing.
- ☐ ☐ 19. **Signature of Owner**, properly attested.

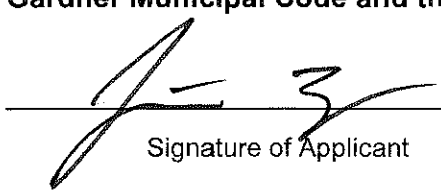
- |     | Yes                      | No                       |   |
|-----|--------------------------|--------------------------|---|
|     | <input type="checkbox"/> | <input type="checkbox"/> | 20. <b>A dated signature and seal of the licensed Land Surveyor responsible for the survey and a note</b> stating: "This survey conforms to the Kansas Minimum Standards for Boundary Surveys."   |
| N/A | <input type="checkbox"/> | <input type="checkbox"/> | 21. <b>Copies of all pertinent exception documents</b> , or a copy of a current American Land Title Association (ALTA) survey, or both.   |
|     | <input type="checkbox"/> | <input type="checkbox"/> | 22. <b>Calculation documents</b> containing the following data: coordinates of the plat boundary and the unadjusted error of closure of the field traverse that established the plat.   |
|     | <input type="checkbox"/> | <input type="checkbox"/> | 23. <b>A statement on the plat concerning prior easement rights</b> as follows: The undersigned proprietor of said property shown on this plat does hereby dedicate for public use and public ways and thoroughfares, all parcels and parts of land indicated on said plat as streets, terraces, places, roads, drives, lanes, parkways, avenues and alleys not heretofore dedicated. Where prior easement rights have been granted to any person, utility or corporation on said parts of the land so dedicated, and any pipes, lines, poles and wires, conduits, ducts or cables heretofore installed thereupon and therein are required to be relocated, in accordance with proposed improvements as now set forth, the undersigned proprietor hereby absolves and agrees to indemnify the City from any expense incident to the relocation of any such existing utility installations within said prior easement. |
|     | <input type="checkbox"/> | <input type="checkbox"/> | 24. <b>A statement on the plat concerning utility easements</b> as follows: An easement or license to enter upon, locate, construct, use and maintain or authorize the location, construction or maintenance and use of conduits, water, gas, sewer pipes, poles, wires, drainage facilities, irrigation systems, ducts and cables, and similar facilities, upon, over and under these areas outlined and designated on this plat as "Utility Easement" or "U/E" is hereby granted to the City with subordinate use of the same by other governmental entities and public utilities as may be authorized by state law to use such easement for said purposes. Utility easements shall be kept clear of obstructions that impair the strength or interfere with the use and/or maintenance of public utilities located within the easement.  |
|     | <input type="checkbox"/> | <input type="checkbox"/> | 25. <b>A statement on the plat concerning drainage easements</b> as follows: An easement or license to enter upon, locate, construct, use and maintain or authorize the location, construction, maintenance or use of conduits, surface drainage facilities, subsurface drainage facilities, and similar facilities, upon, over and through those areas outlined and designated on this plat as "Drainage Easement" or "D/E" is hereby granted to the City. Drainage easements shall be kept clear of obstructions that impair the strength or interfere with the use and/or maintenance of storm drainage facilities.  |
| N/A | <input type="checkbox"/> | <input type="checkbox"/> | 26. <b>Certification of dedication of all streets, highways and other rights-of-way or parcels for public park or other public use</b> , signed by the owners and all other parties who have a mortgage or lien interest in the property.   |

**Written explanations** for any items not checked or checked "No" (attach additional sheets, if necessary):

Items not checked pertain to signatures which will be provided prior to recording the plat and easements

that will be updated as the site is fully designed.

I hereby submit all information required for final plat review. I understand that failure to provide the required information may result in a postponement of my request for review until all information has been submitted. By signing this application, I acknowledge that all public streets and public infrastructure within the plat shall conform to the applicable minimum design standards set forth in the Gardner Municipal Code and the Technical Specifications.

  
Signature of Applicant  
Date



**PLANNING COMMISSION STAFF REPORT**  
**MEETING DATE: FEBRUARY 25, 2020**

**NEW BUSINESS ITEM No. 3C**

**PREPARED BY: KELLY DRAKE WOODWARD, AICP, CHIEF PLANNER**

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**PROJECT NUMBER / TITLE: SP-20-01: Site Plan Review for Frontier Community Credit Union**

---

**PROCESS INFORMATION**

**Type of Request:** Site Plan Review

**Date Received:** January 10, 2020

**APPLICATION INFORMATION**

**Applicant:** Jim Long P.E., Anderson Engineering

**Owner:** Frontier Community Credit Union, Michael Augustine

**Parcel ID:** CF221424-4032 and CF221424-4028

**Location:** On the west side of Moonlight Road approximately 260' south of Madison Street

**REQUESTED ACTION**

The applicant requests approval of a site plan review for a credit union facility on an approximately 1.33 acre lot.

**EXISTING ZONING AND LAND USE**

The subject property is currently vacant and unplatted land zoned C-2 (General Business) District. There is an existing access drive onto N. Moonlight Road that was developed when Casey's General Store was established.

**SURROUNDING ZONING AND LAND USE**

<b>Zoning</b>	<b>Use(s)</b>
<b>North of subject property</b>	
C-2 (General Business) District	Convenience Store
<b>East of subject property</b>	
CP-2 (Planned General Business) District	Multi-tenant Commercial Center
<b>South of subject property</b>	
C-3 (Heavy Commercial) District	Equipment Rental Store
<b>West of subject property</b>	
C-2 (General Business) District	Vacant lot with improved access drive

## **EXISTING CONDITIONS**

The site plan is for the vacant lot south of Casey's General Store along Moonlight Road. The lot will eventually be surrounded by commercial uses. There is single-family residential to the north and west and apartment townhomes to the south.



## **BACKGROUND / HISTORY**

The property was annexed in 1970 under Ordinance Number 1015. Casey's General Store was platted and built in 2014.

## **CONSISTENCY WITH COMPREHENSIVE PLAN**

The existing land use is consistent with the Land Use Plan map of the *Gardner Comprehensive Plan* (specifically the *Gardner Main Street Corridor Plan*, adopted as part of the Comprehensive Plan) that designates this property for Community Mixed Use described as follows:

*Community Mixed Use areas are intended to provide retail and professional services for the everyday needs of the people residing or working in the community. Uses should be limited to those that meet the needs of residents such as civic uses, grocery and retail stores, restaurants, hotels, professional services, and entertainment venues, as well as office and medical commercial uses. Community mixed uses can be located in stand-alone structures, or as part of a small shopping center with multiple uses, and may include complementary residential development that provides opportunities for housing near jobs and essential goods and services. These areas are developed with a focus on pedestrian safety and connectivity and support of alternative transportation options. Vehicular circulation and connectivity is accommodated in a less visually prominent pattern, minimizing the number of access drives and screening parking areas from the public space.*

This Community Credit Union facility provides professional services near residents. The site plan provides for a direct mid-block sidewalk connection to housing areas to the west. Further pedestrian accessibility is provided by the existing trail along Moonlight Road. There is a small parking lot including ADA parking spaces along the front façade (facing Moonlight Road), but otherwise the remainder of the parking and drive-thru lanes are beside or behind the structure. The facility will utilize shared access drives and there is ample vegetation screening parking from public spaces.

## **STAFF ANALYSIS**

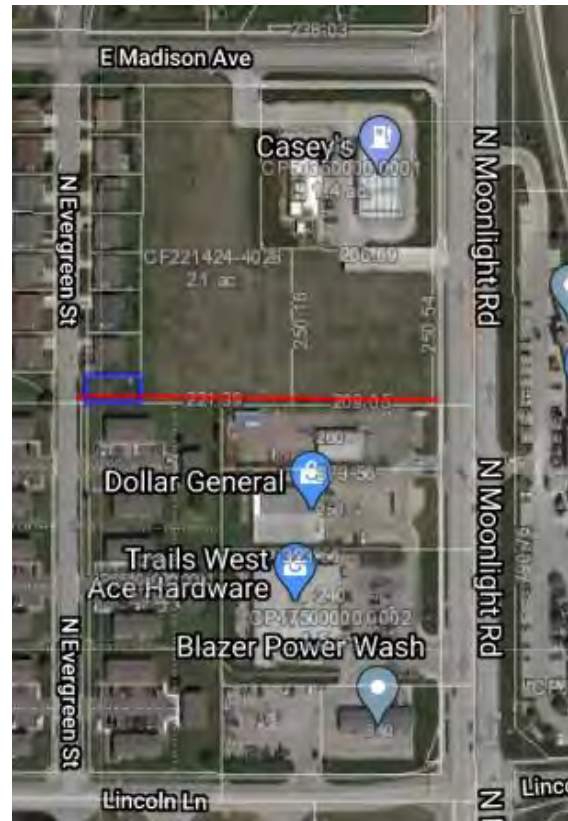
### **SITE PLAN**

#### **17.03.060 (B) Review Criteria:**

1. In general, any site plan in compliance with all requirements of this Code shall be approved.

**Staff Comment:** *The following elements have been added by the applicant for optimal compliance with the Code:*

- *The LDC provides for exceptions to block and connectivity standards. In this case, the block length is slightly over the maximum of 1,000 ft and substantially over the maximum block area of 8 acres. When exceptions for larger blocks apply, the Planning Commission may require pedestrian walkways through blocks. Additionally, the LDC provides that development sites shall include direct sidewalk connections in the most direct manner with adjacent sites where pedestrian connections through public streetscapes or internal access is remote (there are no sidewalks along the existing or proposed access drive). Sidewalk connectivity is needed to the vacant lot to the west as shown on the photo at right. The applicant has, consistent with Staff request, provided for a public access easement containing a 5' sidewalk along the south property boundary of this lot and the adjacent vacant lot. The City will need to pursue the remaining sidewalk connection across the drainage easement (tract outlined in blue) for the adjacent subdivision Madison Park to connect with Evergreen Street (approximate sidewalk location shown in red).*
- *The LDC provides that no use shall provide greater than 20 percent more than the minimum required parking without documented evidence of actual parking demand based on studies of similar uses in similar contexts, and that any parking permitted over 20 percent more than the minimum shall require mitigation of potential impacts through one or more stated strategies. The applicant has provided the attached letter documenting the need for the proposed parking, and is providing additional buffers and vegetation to screen*



*parking (additional foundation landscaping and continuous shrub border along the east access drive).*

*The following elements are recommended conditions of approval to meet LDC standards:*

- Relocate four medium/large shade trees to the area of the right-of-way adjacent to the public trail to meet the requirement for street trees.
- Provide a maintenance agreement for the shared access easements prior to the release of the plat for recording.
- If the rooftop mechanical equipment is not adequately screened by the building parapets, the equipment will be screened in accordance with Sections 17.05.020 (B.1) and 17.08.040 (A.1).

2. In making a determination of compliance, or for site plans accompanying any discretionary review or administrative relief, the review body shall consider whether:

- a. The site is capable of accommodating the buildings, proposed use, access and other site design elements required by the Code and will not negatively impact the function and design of rights-of-way or adjacent property.

**Staff Comment:** *The facility is adequately accommodated on this site and benefits the function and design of adjacent property by providing for the continuation of the shared access drives between Moonlight Road and Madison Street, and providing the mid-block sidewalk connection. The plan also provides for a cross access easement and paved driveway stub that could accommodate an interior vehicular connection to the property to the south in the future. Staff finds this criteria is met.*

- b. The design and arrangement of buildings and open spaces is consistent with good planning, landscape design and site engineering principles and practices.

**Staff Comment:** *The site has ample open spaces and site circulation is adequately accommodated based on the truck turning template provided. The attractive building meets design standards and there is ample landscaping provided. Stormwater management plans will need to be approved before the plat is released for recording and the current version is inadequate to make a determination. This is a recommended condition of approval.*

- c. The architecture and building design uses quality materials and the style is appropriate for the context considering the proportion, massing, and scale of different elements of the building.

**Staff Comment:** *This is a small building that offers a variety of complementary materials and parapet heights to add interest. Staff finds this criteria is met.*

- d. The overall design is compatible to the context considering the location and relationships of other buildings, open spaces, natural features or site design elements.

**Staff Comment:** *The site is beneficially related to adjacent parcels by the shared access drive, public trail along Moonlight Road, and the south mid-block sidewalk connection. The plan also provides for a cross access easement and paved driveway stub that could accommodate an interior vehicular connection to the property to the south in the future. The materials used are compatible with the multi-tenant development across Moonlight which uses brick, stone, metal canopies, and ample glass. Staff finds this criteria is met.*

- e. Whether any additional site-specific conditions are necessary to meet the intent and design objectives of any of the applicable development standards.



**Staff Comment:** *As discussed previously, a recommended condition of approval addresses screening of the rooftop mechanical equipment.*

3. The application meets the criteria for all other reviews needed to build the project as proposed.

**Staff Comment:** *The Traffic and Stormwater Management Plans are pending approval by Public Works. The project will also need to obtain approval from the Johnson County Airport Commission and Board of County Commission. The final plat will also need to be recorded before the building permit is approved. These are all recommended conditions of approval.*

4. The recommendations of professional staff.

**Staff Comment:** *Staff recommends approval based on the findings in this staff report.*

### **STAFF ANALYSIS – INFRASTRUCTURE / OTHER**

#### **WATER, SANITARY SEWER, STORMWATER, ELECTRIC, GAS –**

Existing utilities are located either within or adjacent to the site.

#### **ROADWAY NETWORK, VEHICULAR ACCESS –**

There are no new points of access being proposed for this site. Access off of Moonlight Road will be through an existing drive that will provide access throughout the development. An existing access point onto Madison Street will provide secondary ingress/egress throughout the site as well. A cross-access easement is being provided to connect the property to the south in order to limit the amount of curb cuts onto Moonlight Road and provide a more efficient means of movement of vehicular traffic along the west side of the street. A draft cross-access easement across the two adjacent properties to the south was provided by the applicant for future use.

#### **SIDEWALKS –**

An existing 10' trail is located along the west side of Moonlight Road along with a 5' sidewalk extending along the south side of Madison Street. As mentioned before, another 5' sidewalk is being proposed along the entire south property line to provide for a mid-block connection to the residential development to the west.

### **ATTACHMENTS**

- I. Site Plan sheets (6)
- II. Color elevation sheets (2)
- III. Parking letter
- IV. Application

### **ACTIONS**

Per Section 17.03.010 (G) of the *Gardner Land Development Code*, a review body may take the following actions (or recommend the following actions):

1. Approve the application.
2. Approve the application with conditions or modifications to lessen or mitigate a potential impact from the proposed application.



3. Deny the application.
4. Continue the application to allow further analysis. The continued application shall not be more than 60 days from the original review without consent of the applicant. No application shall be continued more than once by each review body without consent of the applicant.

### **EFFECT OF DECISION**

Site Plan and Design Review – Approval of a site plan and design review shall authorize the applicant to apply for a building permit and other applicable permits. The Director may approve minor amendments to approved site plans and design reviews without the refiling of a new application, but in no event shall the Director approve any change that does not qualify for an administrative site plan, or any change that is different from any condition of approval of the site plan and design review. **An approved site plan and design review shall expire and be of no further effect if an application for a building permit for one or more buildings shown on the site plan is not filed within two years of the approval.** The Planning Commission may grant an extension for up to one additional year. Any other element of the plan not submitted for permits within two years shall expire, unless the application proposes a different schedule that is approved.

### **RECOMMENDATION**

Staff recommends approval of the Site Plan subject to the following conditions:

1. Approval of the Traffic Impact Study and Stormwater Management Plan by the City of Gardner Public Works Department prior to the release of the plat for recording.
2. Review and approval at the Johnson County Airport Commission and Board of County Commission shall be completed prior to the issuance of a building permit.
3. The final plat FP-20-01 shall be recorded with the Johnson County Records and Tax Administration prior to the issuance of a building permit.
4. Relocate four medium/large shade trees to the area of the right-of-way adjacent to the public trail to meet the requirement for street trees.
5. Provide a maintenance agreement for the shared access easements prior to the release of the plat for recording.
6. If the rooftop mechanical equipment is not adequately screened by the building parapets, the equipment will be screened in accordance with Sections 17.05.020 (B.1) and 17.08.040 (A.1).

### **Recommended Motion:**

After review of Application SP-20-01, a site plan for Frontier Community Credit Union, located on the west side of Moonlight Road, approximately 260' south of Madison Street, and site plan dated February 14, 2020, and staff report dated February 25, 2020, the Planning Commission approves the application provided the following conditions are met:

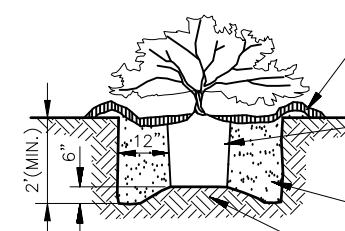
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2. Review and approval at the Johnson County Airport Commission and Board of County Commission shall be completed prior to the issuance of a building permit.
3. The final plat FP-20-01 shall be recorded with the Johnson County Records and Tax Administration prior to the issuance of a building permit.
4. Relocate four medium/large shade trees to the area of the right-of-way adjacent to the public trail to meet the requirement for street trees.






5. Provide a maintenance agreement for the shared access easements prior to the release of the plat for recording.
6. If the rooftop mechanical equipment is not adequately screened by the building parapets, the equipment will be screened in accordance with Sections 17.05.020 (B.1) and 17.08.040 (A.1).






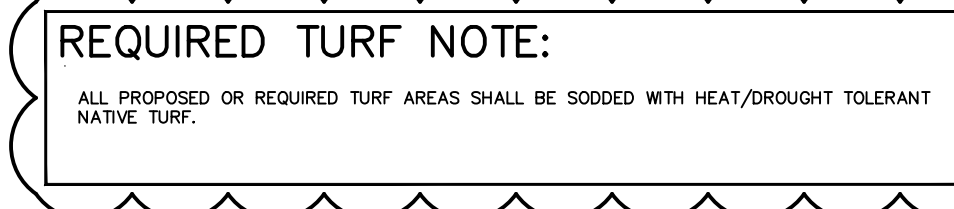
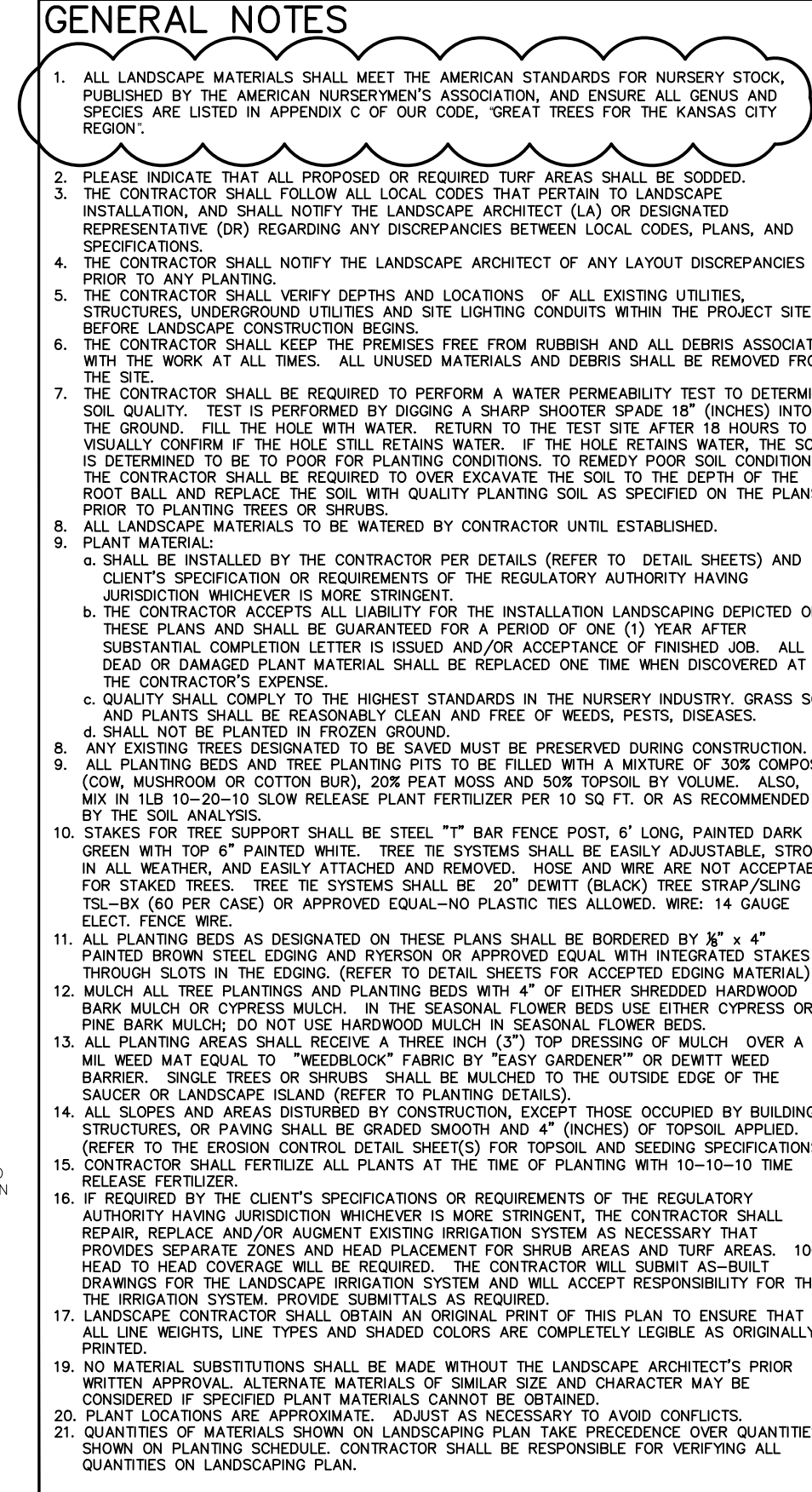
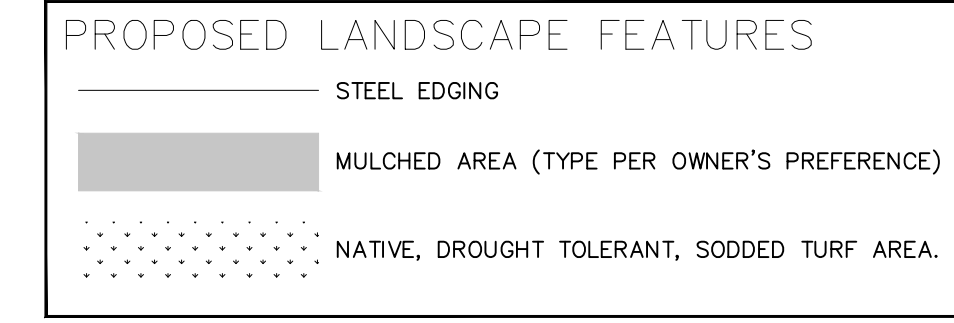
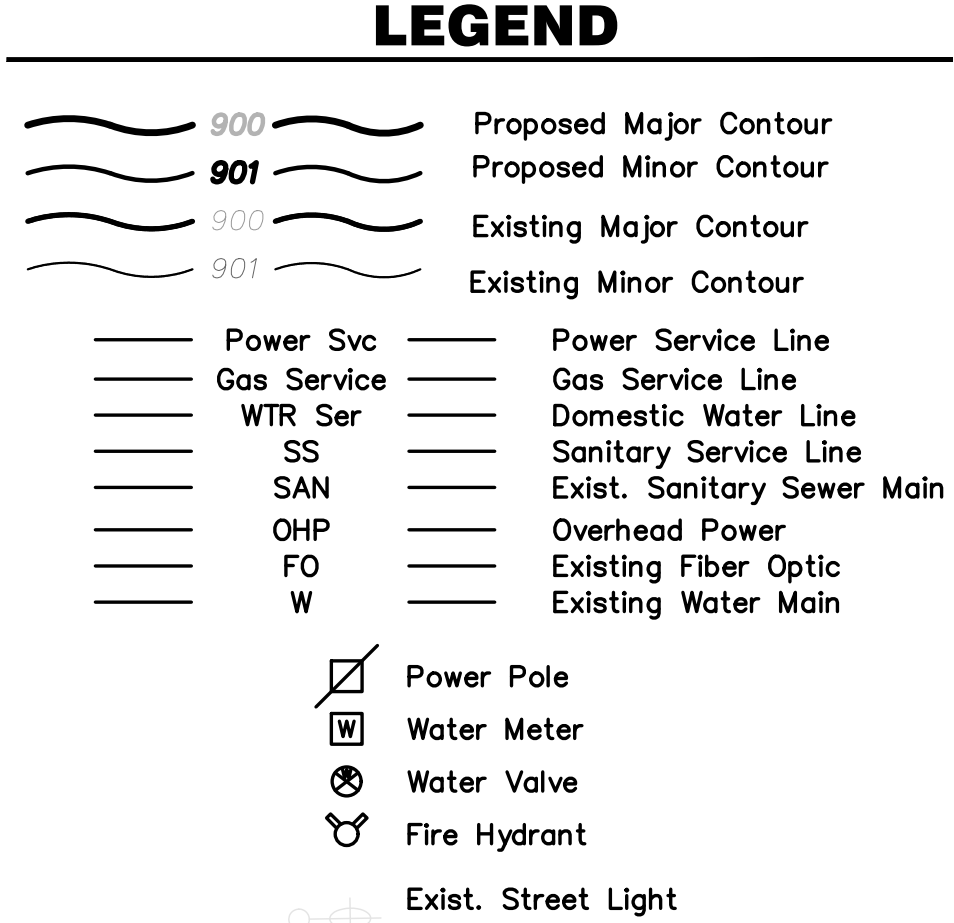
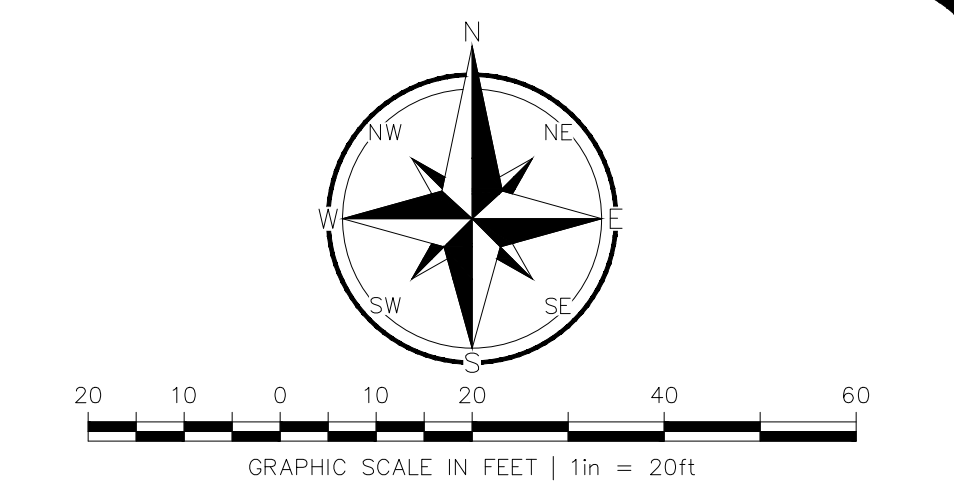
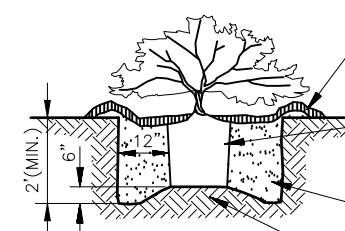
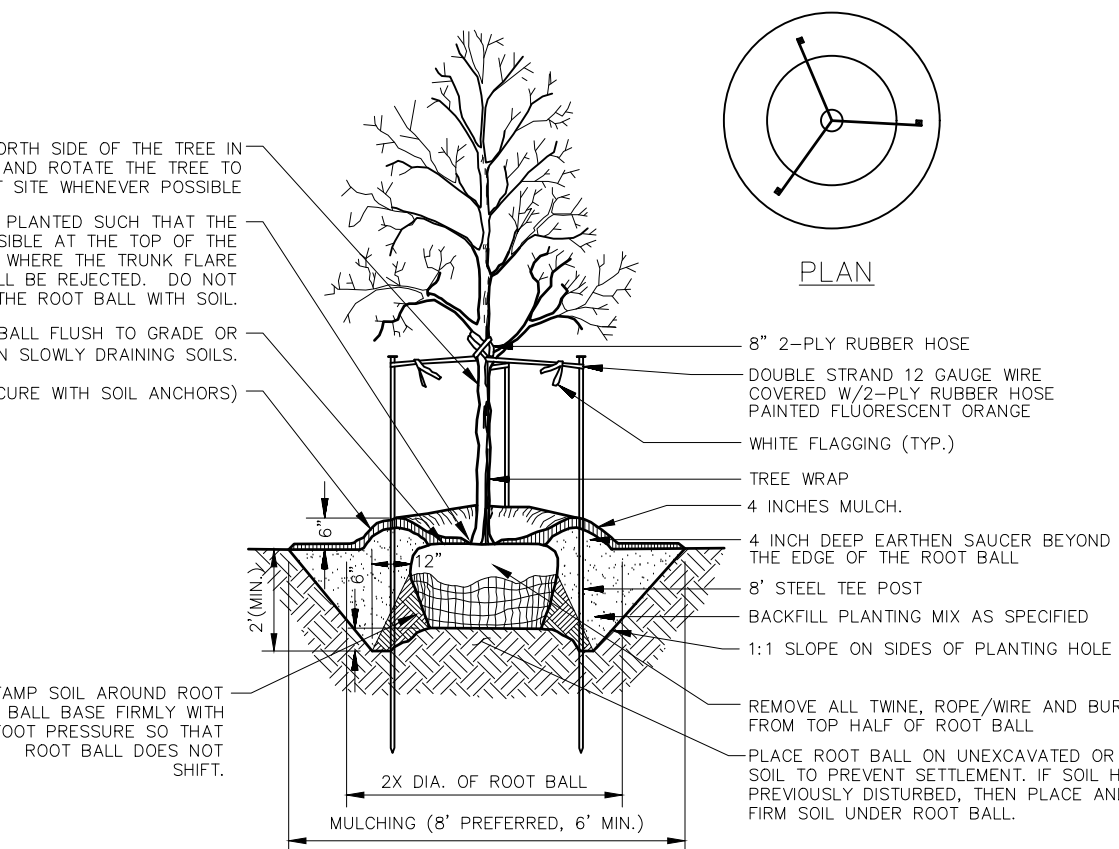
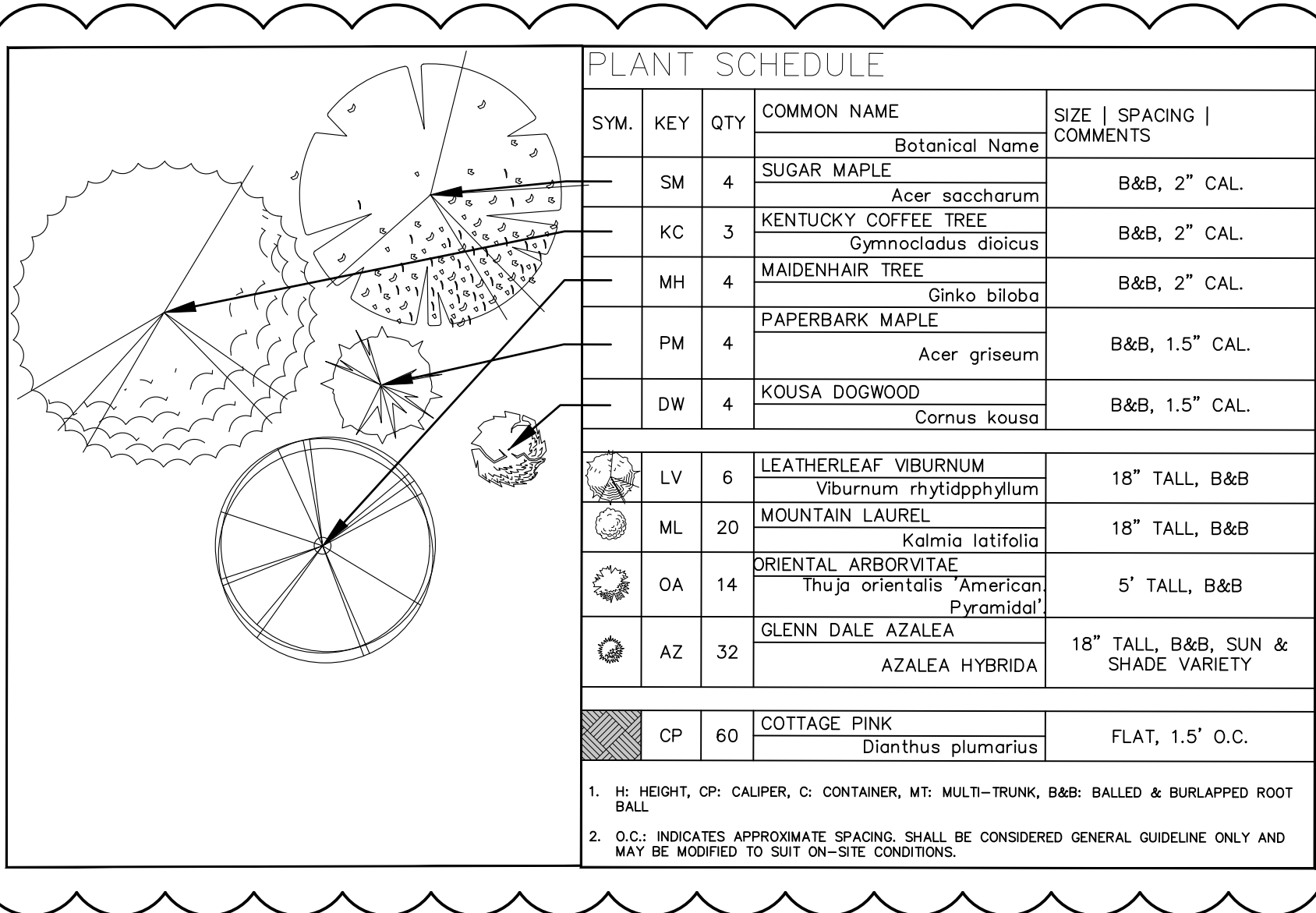




PLANT SCHEDULE					
SYM.	KEY	QTY	COMMON NAME		SIZE   SPACING   COMMENTS
				Botanical Name	
	SM	4	SUGAR MAPLE		B&B, 2" CAL.
			Acer saccharum		
	KC	3	KENTUCKY COFFEE TREE		B&B, 2" CAL.
			Gymnocladus dioicus		
	MH	4	MAIDENHAIR TREE		B&B, 2" CAL.
			Ginkgo biloba		
	PM	4	PAPERBARK MAPLE		B&B, 1.5" CAL.
			Acer griseum		
	DW	4	KOUSSA DOGWOOD		B&B, 1.5" CAL.
			Cornus kousa		
	LV	6	LEATHERLEAF VIBURNUM		18" TALL, B&B
			Viburnum rhytidophyllum		
	ML	20	MOUNTAIN LAUREL		18" TALL, B&B
			Kalmia latifolia		
	OA	14	ORIENTAL ARBORVITAE		5' TALL, B&B
			Thuja orientalis 'American Pyramid'		
	AZ	32	GLENN DALE AZALEA		18" TALL, B&B, SUN & SHADE VARIETY
			AZALEA HYBRIDA		
	CP	60	COTTAGE PINK		FLAT, 1.5" O.C.
			Dianthus plumarius		

1. H: HEIGHT, CP: CALIPER, C: CONTAINER, MT: MULTI-TRUNK, B&B: BALLED & BURLAPPED ROOT BALL

2. O.C.: INDICATES APPROXIMATE SPACING, SHALL BE CONSIDERED GENERAL GUIDELINE ONLY AND MAY BE MODIFIED TO SUIT ON-SITE CONDITIONS.



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A LICENSED KANSAS LANDSCAPE ARCHITECTURE CORPORATION  
KANSAS CERTIFICATE OF AUTHORITY # LA-725 EXPIRES 12/27/2020

REVISONS			DRAWING INFO.	
NO.	DESCRIPTION	BY	DATE	DRAWN BY:
1	PER CITY COMMENTS	NCR	2-3-20	CHECK BY: NCR
2	PER CITY COMMENTS	NCR	2-14-20	LICENSE NO. 896
				DATE: 1/20/2020
				FIELD BOOK:
				JOB NUMBER: 20KC10001
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SHEET NUMBER

1

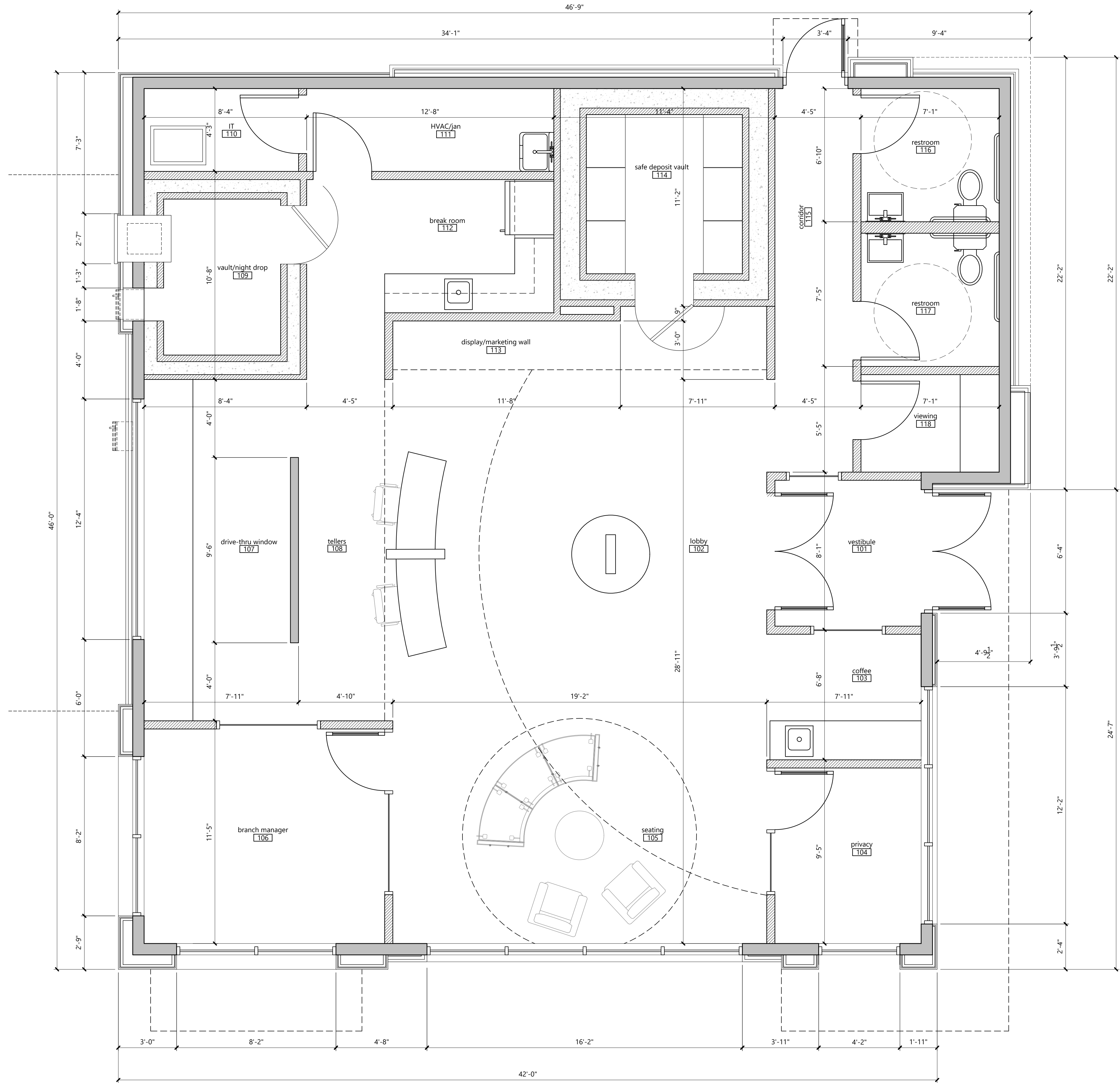
OF 1

Frontier Commerce Park

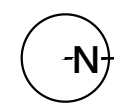
Moonlight Road and E. Madison Street  
Gardner, Kansas

Landscape Plan





1 floor plan  
3/8" = 1'-0"



**design**  
kisha nickell, iida  
principle design studio, llc  
71 se 29th terrace • suite b  
lee's summit • missouri • 64082  
816.897.4941  
principledesignstudio.com  
in association with the architect:  
r h sweers ll, proprietor  
rh sweers architect  
rhs@sweersarch.com  
\*this drawing prepared under my supervision, and i  
disclaim any responsibility for the existing building,  
construction elements, site conditions, or any  
documents which do not bear my signature and seal.

**civil engineer**  
patrick joyce, p.e. & jim long, p.e.  
anderson engineering  
941 w 141st terrace • suite a  
kansas city • missouri • 64145  
816.777.0400  
andersonengineeringinc.com

**mep engineer**  
tbd

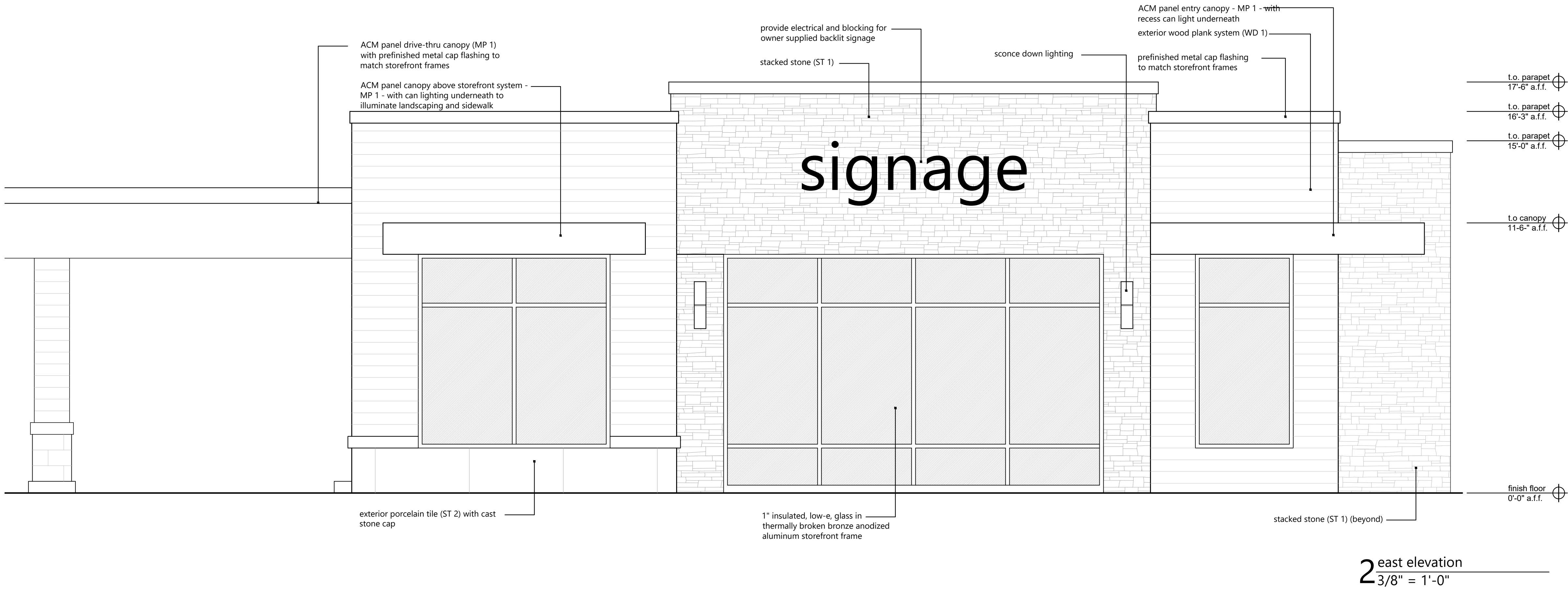
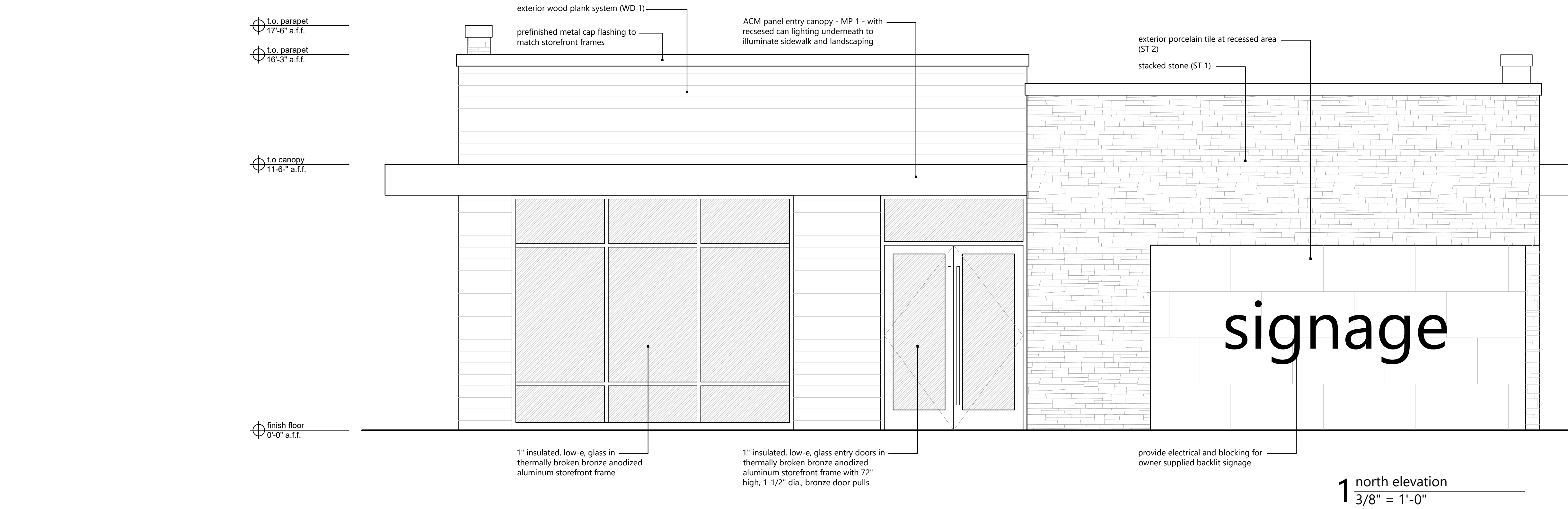
**general contractor**  
ron bratton  
bratton bros. contracting  
6091 woodland road  
shawnee • kansas • 66218  
913.422.7771  
brattonbroscontracting.com

a proposed  
new building for  
**Frontier  
Credit Union**  
southwest corner of  
moonlight & madison  
gardner • kansas

drawing type	planning
project no.	19013
date	01.05.20
rev ##	date
rev 01	02.13.20

sheet number

**A2.1**



**design**

kisha nickell, iida  
principle design studio, llc  
71 se 29th terrace • suite b  
lee's summit • missouri • 64082  
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anderson engineering  
941 w 141st terrace • suite a  
kansas city • missouri • 64145  
816.777.0400  
andersonengineeringinc.com

**mep engineer**

tbd

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new building for

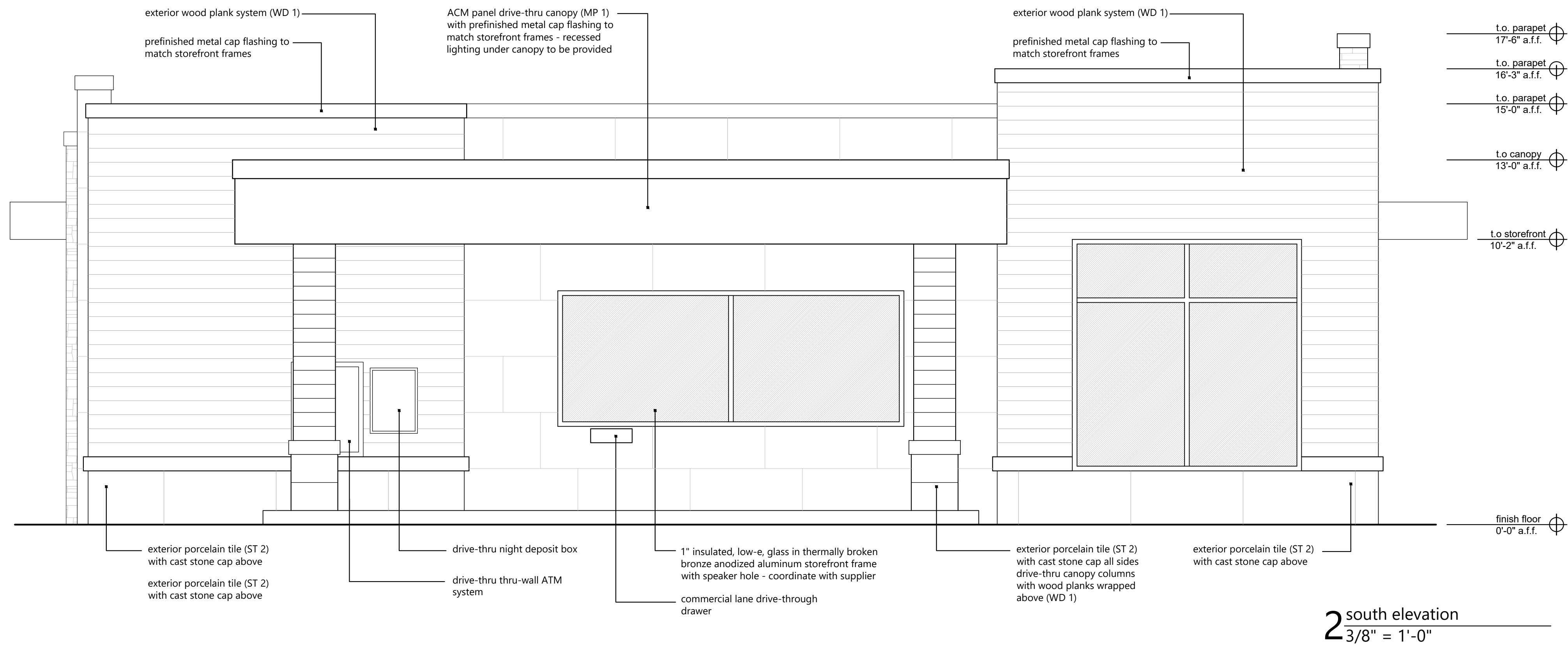
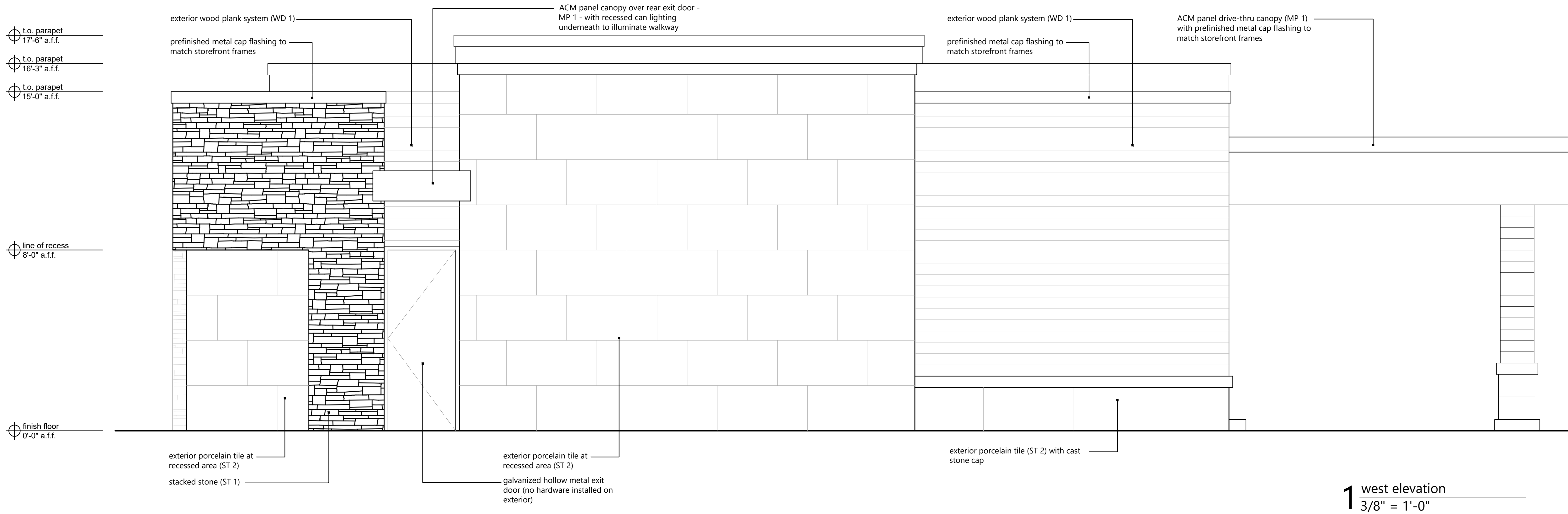
**Frontier  
Credit Union**

southwest corner of  
moonlight & madison  
gardner • kansas

drawing type	planning
project no.	19013
date	01.05.20
rev ##	date
rev 01	02.13.20

sheet number

**A3.1**



design

kisha nickell, iida  
principle design studio, llc  
71 se 29th terrace • suite b  
lee's summit • missouri • 64082  
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anderson engineering  
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andersonengineeringinc.com

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a proposed  
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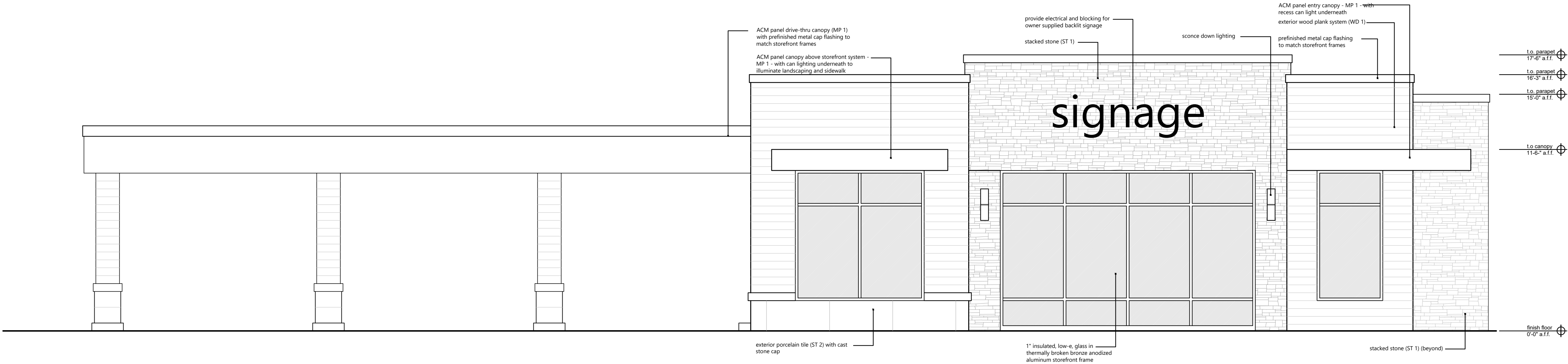
Frontier  
Credit Union

southwest corner of  
moonlight & madison  
gardner • kansas

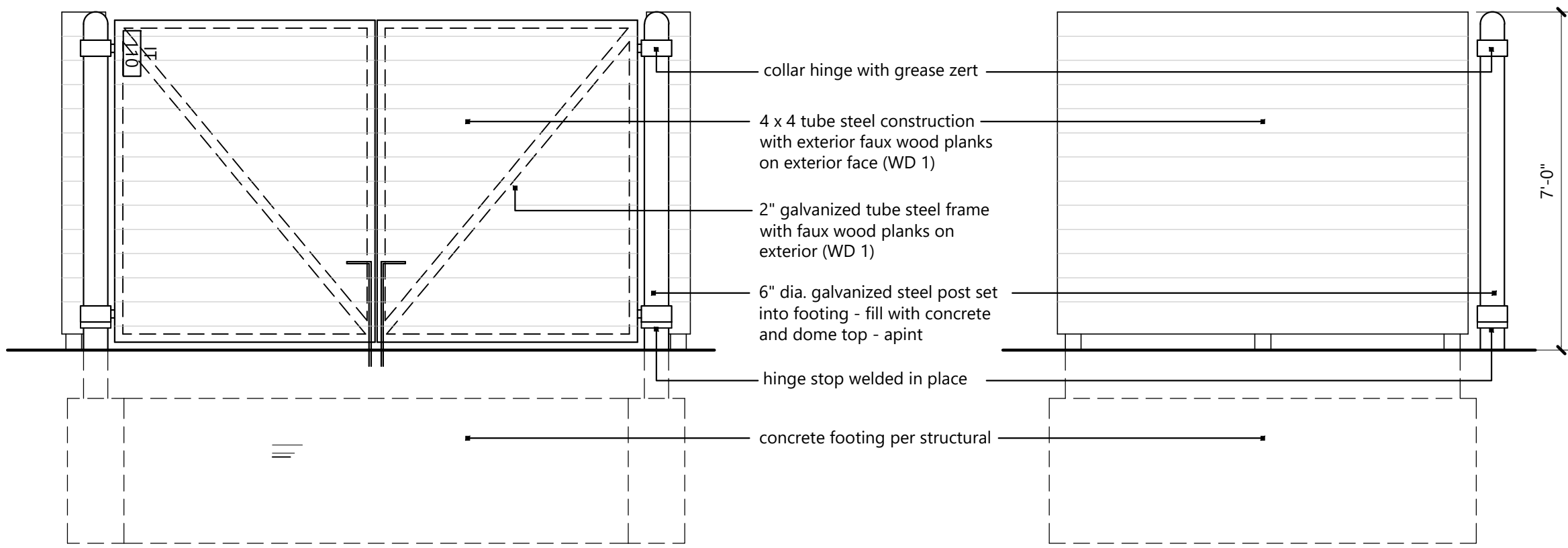
drawing type	planning
project no.	19013
date	01.05.20
rev ##	date
rev 01	02.13.20

sheet number

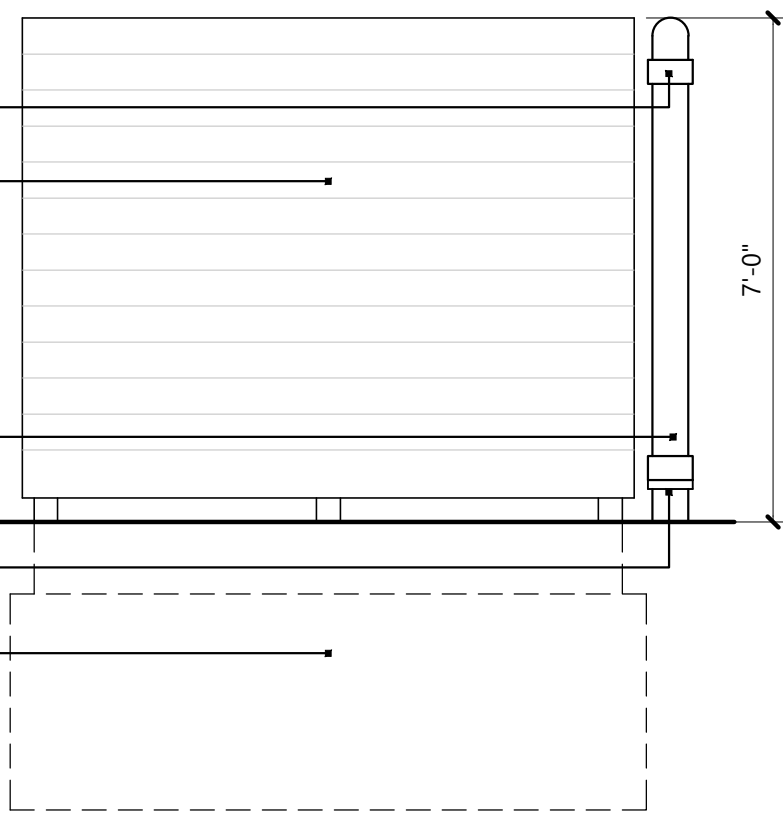
A3.2



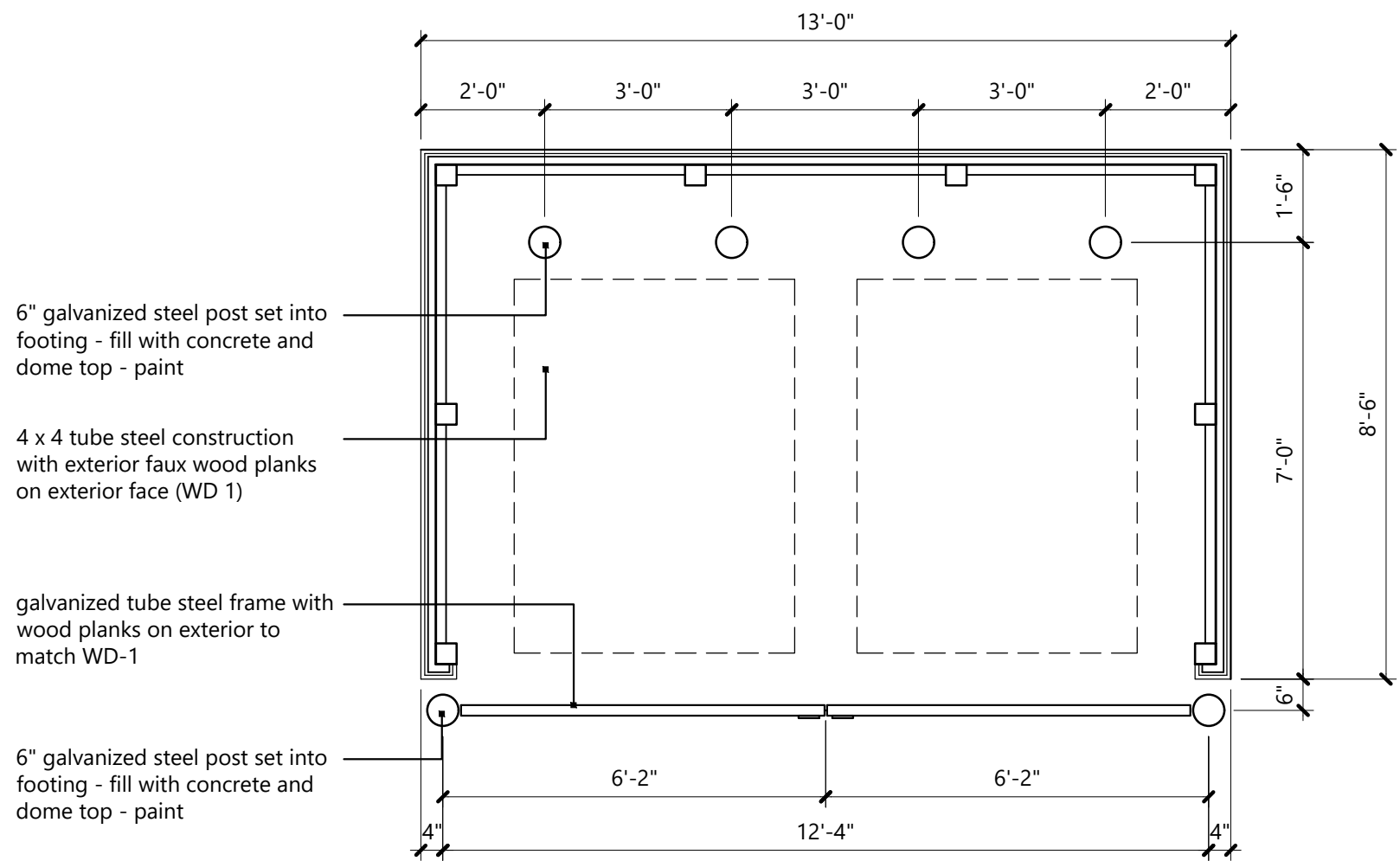
1 full east elevation with drive-thru lanes  
1/4" = 1'-0"



4 trash enclosure front elevation  
3/8" = 1'-0"



3 trash enclosure side elevation  
3/8" = 1'-0"



2 trash enclosure plan  
3/8" = 1'-0"

design

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sheet number

A3.3



exterior building materials:

- MP 1: Metal Canopy panel, color: custom to match 'Frontier Community Credit Union blue' used in logo
- WD 1: Exterior Wood Planking System - Zuri Premium Decking by Royal - co-extruded PVC deck board with a photorealistic wood grain pattern and acrylic capstock layer. Boards are manufactured of nominally 1" thick x 5-1/2" wide profiles available in 12, 16 & 20 feet. Attach to metal stud wall configuration with hat channels. Miter outside corners.
- Color: Chestnut
- ST 1: Stacked Stone with cast stone cap  
Color: Daybreak by Eldorado Stone or similar
- ST 2: Daltile, EC1, Colorbody Porcelain, Color: Bank J101, Size: 24 x 48, Finish: Matte, Installation: 1/3 brick offset
- Glass: 1" insulated clear, low E glass with argon fill - SOLARBAN 60
- Exterior Storefront:  
Thermally broken anodized aluminum  
Color: Bronze Anodized Aluminum
- Hollow Metal Doors & Frames:  
Paint doors and frames to match bronze anodized storefront frames
- Flashing: Pre-finished metal flashing - color: to match bronze anodized storefront frames
- Caulk: To match adjacent material, color: TBD
- Roof: White, TPO, insulated roof
- \*\*note:** all wall vents, etc to match adjacent wall color



1 north elevation  
not to scale



2 east elevation  
not to scale

design

kisha nickell, iida  
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civil engineer

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anderson engineering  
941 w 141st terrace, suite a  
kansas city, missouri 64145  
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mep engineer

tbd

general contractor

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913.422.7771  
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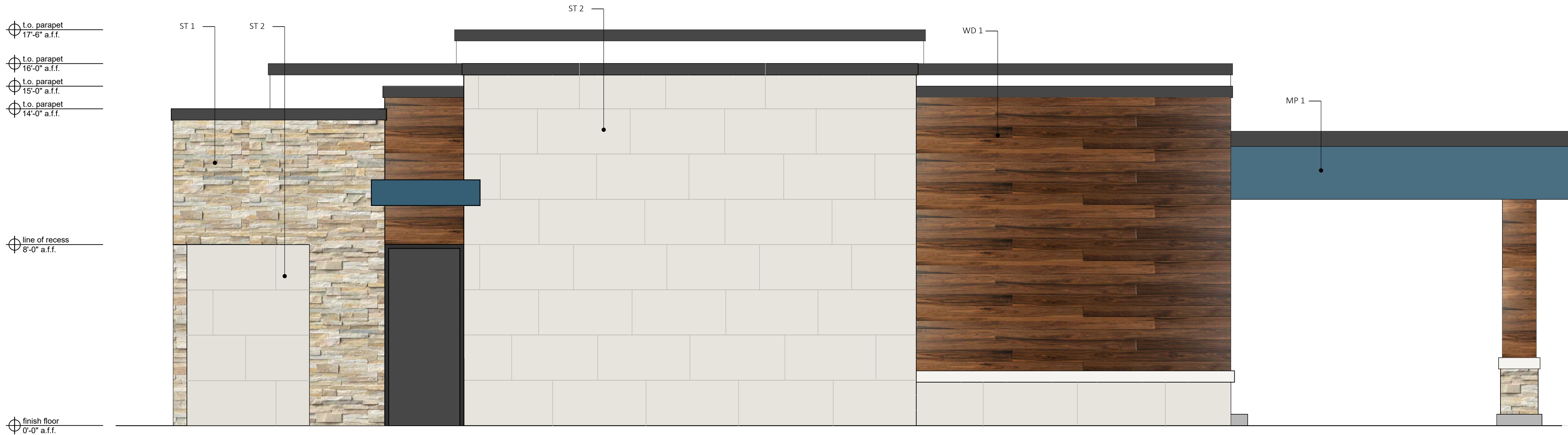
a proposed  
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**Frontier  
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southwest corner of  
moonlight & madison  
gardner, kansas

drawing type	planning
project no.	19013
date	01.05.20
rev ##	date

sheet number

A3.1





1 west elevation  
not to scale



2 south elevation  
not to scale

design

kisha nickell, iida  
principle design studio, llc  
71 se 29th terrace - suite b  
lee's summit, missouri 64082  
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in association with the architect  
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civil engineer

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sheet number

A3.2





690 Eisenhower Rd., Leavenworth, KS 66048  
506 S. 4th St., Leavenworth, KS 66048  
301 Kansas Ave., Fort Leavenworth, KS 66027

February 13th, 2020

City of Gardner  
ATTN: Kellie Drake Woodard, Chief Planner  
120 E. Main St.  
Gardner, KS 66030

RE: Parking Spaces for Frontier Community Credit Union Project

Ms. Woodard:

In reference to your request for clarification, the credit union will staff this location with 5 full-time equivalents (FTEs). The 5 FTE's will initially be composed of 3 full-time employees and 4 part-time employees. This is subject to change based on volume and hours of operation. In the beginning, the hours of operation will be as follows:

Lobby

Monday - Friday: 09:00 AM - 05:00 PM

Drive-Thru

Monday - Friday: 07:30 AM - 06:00 PM

Saturday: 09:00 AM - 01:00 PM

This will match the hours of operation for our other locations, except Fort Leavenworth. However, the management staff is currently considering expanding the credit union's hours by half an hour for both the lobby and drive-thru. Parking spaces will be needed for 5 employees at any given time plus 1 for management staff. Additionally, parking spaces will be needed for members. The credit union currently serves, on average, 30 members per hour per day.

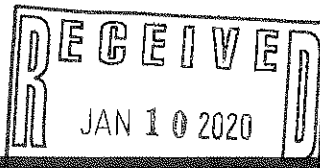
In addition, the credit union is a major supporter of financial literacy. The credit union will be hosting various financial literacy seminars in any given year because of this. As a result, parking will be needed for individuals attending those seminars.

I hope this satisfactorily answers your questions/concerns. If you have any further questions or concerns, please do not hesitate to reach out to me any time at [maugustine@frontierccu.org](mailto:maugustine@frontierccu.org) or (913) 946-1411.

Regards,

Michael J. Augustine  
President/CEO





By \_\_\_\_\_  
**SITE PLAN AND DESIGN REVIEW  
FINAL DEVELOPMENT PLAN  
APPLICATION**

Pre-App Date \_\_\_\_\_  
Fee \$300  
File No. SP-20-01

**OWNER INFORMATION**

Name(s) Frontier Community Credit Union  
Contact Michael Augustine  
Address 690 Eisenhower Road  
City Leavenworth State KS Zip 66048  
Phone 913.651.6575 Email maugustine@frontierccu.org

**APPLICANT/AGENT INFORMATION**

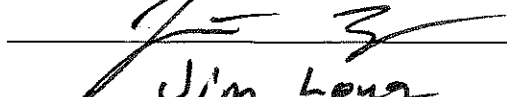
Name(s) Anderson Engineering  
Contact Jim Long, P.E.  
Address 941 W 141st Terrace, Suite A  
City Kansas City State MO Zip 64145  
Phone 816.777.0400 Email jlong@andersonengineeringinc.com

**SITE INFORMATION**

Property Address/Location: Southwest Corner of E. Madison St. and Moonlight Rd. (south and west of Casey's)  
Legal Description (Attach If Necessary) See Attached  
Number of Existing Lots 2 Tracts Number of Proposed Lots 2  
Total Site Area 3.24 ac. Present Zoning C-2  
Proposed Use 0 Present Land Use Vacant  
Proposed Street Design Type(s) & Class N/A  
Proposed Type(s) Open & Civic Space \_\_\_\_\_  
Proposed Frontage Type(s) \_\_\_\_\_  
Proposed Building Types(s) \_\_\_\_\_

**SIGNATURE**

I/We, the undersigned am/are the (owner(s)), (duly authorized agent), (Circle One) of the aforementioned property. By execution of my/our signature, I/we do hereby officially apply for a site plan and design review or final development plan as indicated above.

Signature(s):  Date 1/10/2020  
Jim Long Date \_\_\_\_\_



## SITE PLAN AND DESIGN REVIEW, AND FINAL DEVELOPMENT PLAN APPLICATION CHECKLIST

### APPLICATION SUBMITTAL REQUIREMENTS

- |            | Yes                                 | No                       |  |
|------------|-------------------------------------|--------------------------|--|
|            | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 1. Complete application packet   |
|            | <input type="checkbox"/>            | <input type="checkbox"/> | 2. Application fee <i>(To Be Mailed)</i>   |
|            | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 3. <del>3</del> 10 complete sets of full sized plans printed including color elevations, folded  |
|            | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 4. Digital copies (PDF) of the completed application, plans, and legal description   |
| <i>N/A</i> | <input type="checkbox"/>            | <input type="checkbox"/> | 5. Copy of all covenants and restrictions applicable to the development, if applicable.  |
|            | <input type="checkbox"/>            | <input type="checkbox"/> | 6. Sign posting affidavit.   |
| <i>N/A</i> | <input type="checkbox"/>            | <input type="checkbox"/> | 7. Evidence of the establishment of the agency for the ownership and maintenance of any common open space and all assurances of the financial and administrative ability of such agency or document dedicating the facilities to the city. |
| <i>N/A</i> | <input type="checkbox"/>            | <input type="checkbox"/> | 8. Deeds of dedication for all rights-of-way or easements required as a result of preliminary development plan approval if conveyance thereof is not to be made by plat or by the filing of the final development plan.                    |
| <i>N/A</i> | <input type="checkbox"/>            | <input type="checkbox"/> | 9. Evidence of satisfaction of any conditions of the preliminary development plan approval which were conditions precedent to consideration of the final development plan.   |
|            | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 10. Final Stormwater Management Plan (2 printed and 1 digital copy) <i>Sent to Mark P</i>  |
| <i>N/A</i> | <input type="checkbox"/>            | <input type="checkbox"/> | 11. Final Traffic Impact Study (TIS) as required by the Access Management Code. (2 printed and 1 digital copy)   |

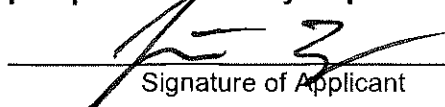
### PLAN REQUIREMENTS

- |                                     |                          |  |
|-------------------------------------|--------------------------|--|
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 1. Vicinity map to scale with north point showing railroads, major streams or rivers, and public streets in the vicinity of the site. (Suggested scale of 1" = 1000'.)   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 2. Boundary lines of the subdivision shall be enclosed with one continuous bold line, showing approximate dimensions (bearings and distances).   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 3. Scale, legend, and north arrow clearly shown, with orientation at top or left as north (not less than 1"=100' scale).   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 4. Dates of plan preparation and/or plan revisions.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 5. Ownership, zoning, and land use of the site and surrounding properties within 200 feet; both existing and proposed.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 6. Names, addresses, and phone numbers of all companies, firms, or individuals involved in the preparation of the plan (i.e. developer, property owner, architect, landscape architect, planner, engineer, surveyor, etc.) |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> | 7. Setback lines: building and parking with dimensions in feet.  |

- |                                     | Yes                      | No                       |   |
|-------------------------------------|--------------------------|--------------------------|---|
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |                          | 8. <b>Setback lines:</b> building and parking with dimensions in feet.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |                          | 9. <b>Lots and tracts identified clearly</b> , with blocks numbered or lettered boldly and clearly in the center of the block, and lot dimensions with bearings and distances, and area in square feet and acres.   |
| N/A                                 | <input type="checkbox"/> | <input type="checkbox"/> | 10. <b>Note on the plan indicating intended ownership, purpose, and maintenance responsibilities</b> for any parcels labeled as tracts.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |                          | 11. <b>Setback lines:</b> building and parking with dimensions in feet.   |
| N/A                                 | <input type="checkbox"/> | <input type="checkbox"/> | 12. <b>Location of existing open space, alleys, parks, streams, ponds, vegetation</b> or other similar features within plan area, and whether they are to be retained or removed.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |                          | 13. <b>Existing utilities</b> , including sanitary sewer, force main, water main, gas mains, culverts and storm sewer pipe, street lights, electric conduits, and invert elevations of sewers at points of proposed connection.   |
| N/A                                 | <input type="checkbox"/> | <input type="checkbox"/> | 14. <b>Proposed street network</b> , including right-of-way, bearings, tangents, dimensions, and horizontal and vertical curvature data along the centerline of each street.  |
| N/A                                 | <input type="checkbox"/> | <input type="checkbox"/> | 15. <b>All public streets within the plan conform</b> to the applicable minimum design standards set forth in the Land Development Code and Technical Specifications. Design of crosswalks, on street parking, shoulder, pavement and lane dimensions.  |
| N/A                                 | <input type="checkbox"/> | <input type="checkbox"/> | 16. <b>Intersection site distance analysis.</b>   |
| N/A                                 | <input type="checkbox"/> | <input type="checkbox"/> | 17. <b>Driveways, parking lots and stalls, aisles, and loading</b> and service areas and docks and dimensions.  |
| N/A                                 | <input type="checkbox"/> | <input type="checkbox"/> | 18. <b>Median breaks and turning lanes</b> , including sizes and radii; both existing and proposed.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |                          | 19. <b>Vehicle maneuvering/turning templates</b> reflecting the site can accommodate a minimum SU-30 class vehicles (for emergency access to all areas of the site), and the appropriate site-design vehicle for any other special areas of the site (such as delivery or dock areas, etc.), as necessary. <i>To Be Enclosed</i>                                      |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |                          | 20. <b>Existing and proposed sidewalks</b> and/or trail locations including proposed widths.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |                          | 21. <b>Proposed utilities</b> , including approximate location of sanitary sewer, water main, and street lights.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |                          | 22. <b>Existing and proposed easements</b> with dimensions. Existing easements shall be labeled with book and page number.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |                          | 23. <b>Any area within a federally designated floodplain.</b> Location, stations, and elevations of the 100-year floodplain within the plan area and 100-year elevations at rear lot corners adjacent to FEMA and Shaded Zone X floodplains. The source of the floodplain information shall be clearly labeled (example: FIRM, Map #20091C0041D, September 27, 1991). |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |                          | 24. <b>Stream corridor boundary</b> and dimensions.   |
| N/A                                 | <input type="checkbox"/> | <input type="checkbox"/> | 25. <b>Phasing Plan</b> , if applicable.  |

- |     | Yes                                 | No                       |  |
|-----|-------------------------------------|--------------------------|--|
| N/A | <input type="checkbox"/>            | <input type="checkbox"/> | 26. <b>Planned amenities</b> , such as fountains, art, outdoor seating, waste receptacles, etc.  |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 27. <b>Any buildings within the plan area</b> which are existing or proposed, with status indicated including dimensions (i.e. to remain, remodel, new, to be demolished as part of Phase 2, etc.).  |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 28. <b>Distances between all buildings</b> , between buildings and property lines, and between parking areas and property lines.   |
| ??  | <input type="checkbox"/>            | <input type="checkbox"/> | 29. <b>Existing Topography and Proposed Grading</b> of the area contained in the plan area and within 20 feet of the boundary shown by 2-foot contour intervals. Contour lines shall be legible but not overpowering.  |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 30. <b>Building elevations</b> depicting the architectural style, size, exterior construction materials, and colors for each type of building proposed, and dimensions. If an architectural theme is planned, elaboration on the intent and extent of the scheme and details shall be provided. Elevations shall provide sufficient information to determine relationships between various elements, building height, proportion, bedroom counts, approximate square footage, etc. Rooftop and ground-mounted mechanical equipment shall be shown on elevations. |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 31. <b>Screen walls, fences, trash enclosures, and mail kiosks</b> (existing and proposed), including location, height, and materials.   |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 32. <b>Table indicating lots, land areas, buildings, number of stories, building coverage, and all other quantities relative</b> to the submitted plan that are required to determine compliance with City codes. For commercial buildings, indicate service floor areas and number of tenant spaces, if applicable. For residential buildings, indicate dwelling units; if multiple building types, provide an additional table indicating dwelling units by building type.   |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 33. <b>Table indicating required and proposed parking spaces.</b>  |
|     | <input type="checkbox"/>            | <input type="checkbox"/> | 34. <b>Landscaping plan and table</b> indicating all proposed landscaping, noting common and botanical names, numbers, and planting sizes. Note all other areas to be sodded.  |
|     | <input type="checkbox"/>            | <input type="checkbox"/> | 35. <b>All exterior sign locations.</b> Include elevations and details. <i>not ready</i>   |
| N/A | <input type="checkbox"/>            | <input type="checkbox"/> | 36. <b>All outside lighting facilities:</b> Location, height, wattage, and type including shielding, for buildings, parking lots and outdoor storage areas.  |
| N/A | <input type="checkbox"/>            | <input type="checkbox"/> | 37. <b>Outdoor storage areas</b> , including location, dimensions and design.  |
| N/A | <input type="checkbox"/>            | <input type="checkbox"/> | 38. <b>Planned amenities</b> , such as fountains, art, outdoor seating, waste receptacles, etc.  |
|     | <input checked="" type="checkbox"/> | <input type="checkbox"/> | 39. <b>Preliminary design and location of all proposed storm drainage</b> conveyance, detention and treatment facilities and locations of existing drainage facilities.  |

I hereby submit all information required for a site plan and design review, or final development plan application review. I understand that failure to provide the required information may result in a postponement of my request for review until all information has been submitted.

  
Signature of Applicant

  
Date



### OWNER AFFIDAVIT

I/WE Frontier Community Credit Union, hereby referred to as the "Undersigned", being of lawful age, do hereby on this 13th day of February, 2020, make the following statements to wit:

1. I/We the Undersigned, on the date first above written, am/are the lawful owner(s) in fee simple absolute of the following described real property:

See "Exhibit A, Legal Description" attached hereto and incorporated herein by reference.

2. I/We the undersigned, have previously authorized and hereby authorize Anderson Engineering and/or Kisha Nickell, Principle Design Studio (Herein referred to as "Applicant"), to act on my/our behalf for the purpose of making application with the City of Gardner, regarding To Be Determined (common address), the subject property, or portion thereof. Such authorization includes, but is not limited to, all acts or things whatsoever necessarily required of Applicant in the application process. I/We further attest that I/We agree to be legally bound by the application made on our behalf by applicant and the resultant action upon such application by the City of Gardner.

3. It is understood that in the event the Undersigned is a corporation or partnership then the individual whose signature appears below for and on behalf of the corporation or partnership has in fact the authority to so bind the corporation or partnership to the terms and statements contained within this instrument.

IN WITNESS THEREOF, I, the Undersigned, have set my hand and seal below.

[Signature]  
Owner

\_\_\_\_\_  
Owner

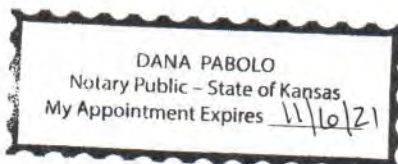
STATE OF KS  
COUNTY OF Johnson

The foregoing instrument was acknowledged before me on this 13<sup>th</sup> day of February, 2020, by

Dana Pabolo

My Commission Expires: 11/16/21

[Signature]  
Notary Public



---

**PROJECT NUMBER / TITLE: CPA-20-01 Amend the *City of Gardner Comprehensive Plan* and adopt the *Gardner Destination Downtown Plan*.**

---

**BACKGROUND**

In April of 2018, the Governing Body approved Comprehensive Plan amendments to incorporate the *Gardner Main Street Corridor Plan* and *Gardner Main Street Market Analysis*. This plan provides specific policy guidance for the entire Main Street Corridor through the City and extending into adjacent neighborhoods and development areas. It focuses on the big picture and crafting a vision that integrates transportation, land use and the environment while engaging the community in a collaborative dialogue. This Sustainable Places Plan was largely funded through the Planning Sustainable Places program with the Mid-America Regional Council.

The *Gardner Destination Downtown Plan* is the second level of planning in the Planning Sustainable Places program, and was also largely funded through that program. It focuses on project development and implementation activities for an 8-block study area of Downtown (and including Cornerstone Park) so that implementation projects can move closer to readiness for preliminary engineering. This plan also includes meaningful public engagement. The premise behind both projects is to build on the role of the corridor as a major vehicular transportation thoroughfare and facilitate its evolution to support multi-modal transportation options and vibrant public spaces that are attractive for redevelopment and revitalization efforts.

Titled *Gardner Destination Downtown: A Placemaking and Mobility Enhancement Project*, the plan provides the following information:

1. *Project understanding* – background, purpose, description of project area, and summary of key issues including the need for new, flexible public spaces; streetscape improvements to knit the community together; improved ADA accommodations; and the location of a mobility hub in Downtown Gardner.
2. *Existing conditions analysis* – an examination of existing green spaces and opportunities for improvement; and the condition of Downtown streetscapes that are currently oriented more to vehicles than pedestrians.
3. *Planning process* – description of planning activities, leadership structure, project partners and stakeholders, and public engagement opportunities (such as the Benchmarking Bus Tour, attendance at community events, online surveys, Walk & Roll Inventory, and public meetings).
4. *Plan concepts* – visualizations and descriptions of recommended improvements for public spaces, streetscapes, a mobility hub, and ADA facilities/parking. A concept plan for residential and mixed-use infill is included along Center Street.
5. *Implementation options and scenarios* – includes planned improvements for *Cornerstone Park* (multi-purpose building, inclusive playground, interactive water feature, additional pavilions) and new *City Hall Plaza* (public market shed and plaza with green infrastructure), connected by a new *Civic Green* (botanical gardens, paths, overhead trellises, a performance structure, plaza, and other furnishings). The planned streetscape improvements are categorized based on classification as a primary, secondary, or tertiary

street, and include recommendations for street reconstruction, sidewalk and ramp improvements, a median on Main Street, road tables on Shawnee and Washington Streets, parking reconfiguration, off-street bike facilities along Center Street, and streetscape elements.

## **PROCESS REQUIREMENTS**

Kansas statutes (K.S.A 12-747) grant planning commissions the authority to create and adopt a comprehensive plan by resolution. Before adopting an amendment to the comprehensive plan, the planning commission shall hold a public hearing, notice of which shall be published in the official city newspaper 20 days prior to the date of the hearing. Upon adoption of the plan amendments by resolution, a certified copy of the plan amendments and a written summary of the hearing shall be submitted to the governing body. No comprehensive plan amendment shall be effective unless approved by the governing body. The governing body may approve the recommended comprehensive plan by ordinance.

A notice of the public hearing was published in the Gardner News on Tuesday, February 4, 2020. If the Planning Commission adopts the attached resolution and forwards a recommendation to the Governing Body to approve the amendments and adopt the plan and market analysis, the next steps will be to provide notice to the County and any townships affected, up to 20 days prior to the action by the Governing Body.

## **ATTACHMENTS**

- I. Draft plan
- II. Draft annotated Comprehensive Plan amendment
- III. Planning Commission Resolution

## **RECOMMENDATION**

Staff recommends the Planning Commission hold a public hearing and adopt Planning Commission Resolution No. PC-20-01 amending the Cover Page, Chapter 1 Introduction, Chapter 3 Public Participation, Chapter 4 Vision, Goals & Objectives, Chapter 5 Land Use & Development, Chapter 9 Image & Identity, Chapter 10 Sustainability Framework, and Chapter 11 Implementation of the *City of Gardner Comprehensive Plan*, and adopting and incorporating by reference the *Gardner Destination Downtown* plan, and forward a recommendation of approval to the Governing Body.

## **Recommended Motion:**

Motion to adopt Planning Commission Resolution No. PC-20-01 to amend the following portions of the *City of Gardner Comprehensive Plan*: Cover Page, Chapter 1 Introduction, Chapter 3 Public Participation, Chapter 4 Vision, Goals & Objectives, Chapter 5 Land Use & Development, Chapter 9 Image & Identity, Chapter 10 Sustainability Framework, and Chapter 11 Implementation and adopt and incorporating by reference the *Gardner Destination Downtown: A Placemaking and Mobility Enhancement Project* plan, and recommend the Governing Body approve the amendments to the *City of Gardner Comprehensive Plan*, and adopt and incorporate by reference the *Gardner Destination Downtown: A Placemaking and Mobility Enhancement Project* plan.



# GARDNER DESTINATION DOWNTOWN

A PLACEMAKING AND MOBILITY ENHANCEMENT PROJECT





# ACKNOWLEDGMENTS

## MAYOR

Steve Shute

## CITY STAFF

Larry Powell - Business and Economic Development Director

Kelly Drake Woodward - Chief Planner

Michelle Leininger - Principal Planner

Bob Case - Planner

James Pruetting - City Administrator

Michael Kramer - Public Works Director

Jason Bruce - Parks and Recreation Director

Matt Wolff - Finance Director

Gonzalo Garcia - Utilities Director

## CITY COUNCIL

Todd Winters

Rich Melton

Mark Baldwin

Randy Gregorcyk

Tory Roberts

## PLANNING COMMISSION

Scott Boden

Steve McNeer

Adrianna Meder

Eric Ford

Kiva Simmons-Lee

Eric Hansen

Kacy Deaton

## TECHNICAL ADVISORY

A.J. Farris - KCATA

Shawn Strate - KCATA

Josh Powers - Johnson County

## STEERING / IMPLEMENTATION COMMITTEE

Aaron Batterbee

Adrianna Meder

Allen Vonderschmidt

Barbara Coleman

Beth Hines

Bill Danner

Danetri Herbert

David Rhodes

Donna Pearce

Ed Reschke

Eric Gourley

Debbie Frizzell

Jackie Garden

Laura Gourley

Jason Camis

Joe Gardenhire

Julie Aldrige

Karen Liu

Kim Liu

Cris Lowe

Mary Bush

Mary Nelson

Sheri Barber

Shirley Brown Van-Arsdale

Steve Hines

Sylvia Fernald

Tracy Lane

Linda Yardley

## PLANNING TEAM

### CONFLUENCE

Terry Berkbuegler

Matt Evett

Marshall Allen

Avery Nichols



Vicki Noteis

### WILSON & COMPANY

Jim Townsend

Drew Pearson



Doug Norsby





# FORWARD

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This project is supported through a grant from the Mid-America Regional Council's (MARC) Planning Sustainable Places Initiative – a regional program funded by the state-allocated Surface Transportation Program (Livable Communities Pilot) and intended to assist communities to explore transportation network improvements that enhance the quality of life and support long-term community growth.

**DRAFT**



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# 1 PROJECT UNDERSTANDING



## 1.1 PROJECT BACKGROUND

The City of Gardner, with assistance from the Mid-America Regional Council (MARC), has begun the process of fine-tuning the actionable items laid out in the 2018 Gardner Main Street Corridor Plan to ensure the future prominence, vibrancy and function of Downtown Gardner. In the Main Street Corridor Plan, a series of recommendations were developed with the intent to be implemented as funding and opportunities became available. These recommendations were categorized into four sections:

- Transportation and Corridor Function
- Economic Development
- Quality of Life
- Green Solutions

The Gardner Destination Downtown Plan focuses on elements under each of those recommendation categories and through a collaborative and multi-faceted design approach, recommends specific projects and interventions that will achieve the goal of transforming Downtown Gardner into the true heart of the City.

### PLANNING SUSTAINABLE PLACES

The goals of Planning Sustainable Places projects such as the Gardner Destination Downtown Plan include:

- Explore transportation network improvements
- Enhance the quality of life for the community
- Support long-term community growth
- Broaden public understanding and involvement of community in sustainability through partnerships
- Develop new tools, policies and practices to enhance decision making and neighborhood vibrancy
- Demonstrate new models that help transform the ways neighborhoods and communities develop
- Promote long-term physical and economic resilience

The plan is organized into six chapters including the Appendix.

#### 1. Project Understanding

Provides an overall background of the plan and its relevance.

#### 2. Existing Conditions

Analyzes the physical and social aspects of the study area.

#### 3. Planning Process

Chronicles the public input process that took place.

#### 4. Concepts

Outlines concepts for public spaces and streetscapes.

#### 5. Implementation

Recommends various phasing and implementation strategies.

An Appendix is included at the end of this plan to provide supporting resources and images that were gathered during the entirety of the process.



Figure 1.01 - Main and Elm Intersection in Downtown Gardner



Figure 1.02 - Businesses along Main Street

## 1.2 PROJECT PURPOSE

The purpose of the Gardner Destination Downtown Plan is to form a coalition of ideas and support needed to create new dynamic public spaces and an enhanced Downtown streetscape to revitalize the central core of the City. The strategies and action steps of the plan build on the role of the corridor as a major transportation thoroughfare and facilitate its evolution to support multi-modal transportation options and vibrant public spaces that attract redevelopment and revitalization efforts. It is a high-level plan that addresses key issues in a connected way but does not provide detailed design or construction specifics. The recommendations in this plan contain both short-term and long-term phasing strategies for the public spaces and for the streetscape.

A fundamental component of the streetscape design will be the consideration of ADA compliance. Business access, ramping, crosswalk markings and signalization were all analyzed as a part of this study and a list of recommendations can be found later in this plan.



### GOALS

#### ***PUBLIC SPACES: CREATE A HEART IN DOWNTOWN***

- Define the users and program of the spaces.
- Provide a flexible and interactive commons to promote social inclusion and civic identity.
- Relate public spaces to the surrounding context and properties.
- Define the experience of the spaces.
- Honor the community's aspirations and vision.
- Respect the site and budget.

#### ***STREETSCAPES: REINFORCE GARDNER'S DOWNTOWN IDENTITY***

- Define the character of the streetscape.
- Accommodate a balance of vehicular and non-vehicular activity.
- Ensure that the design is authentic to Gardner.
- Promote pedestrian and vehicular safety.
- Recommend technically viable solutions.
- Improve conditions for those with mobility challenges in navigating Downtown.



Figure 1.03 - Character images for dynamic public spaces



## 1.3 PROJECT AREA

Gardner, located in the southwest corner of Johnson County, is poised to grow in population as the Kansas City metropolitan area continues to expand outward.

The project area for this plan includes a 9-block zone in the Downtown core. Extents include Sycamore Street to the east, Center Street to the west, Warren Street to the South and the extents of Cornerstone Park to the north.

Main Street and Elm Street represent the two most important streets in the Downtown core and the intersection of these two streets, represents the physical and historical center of the City.

Several prominent community assets are located in the study area including, but not limited to, City Hall, Gardner Elementary School, First Presbyterian Church, Cornerstone Park, the Gardner Senior Center and many small businesses. Adjacent assets include the Johnson County Fairgrounds, Wheatridge Middle School and several residential neighborhoods.

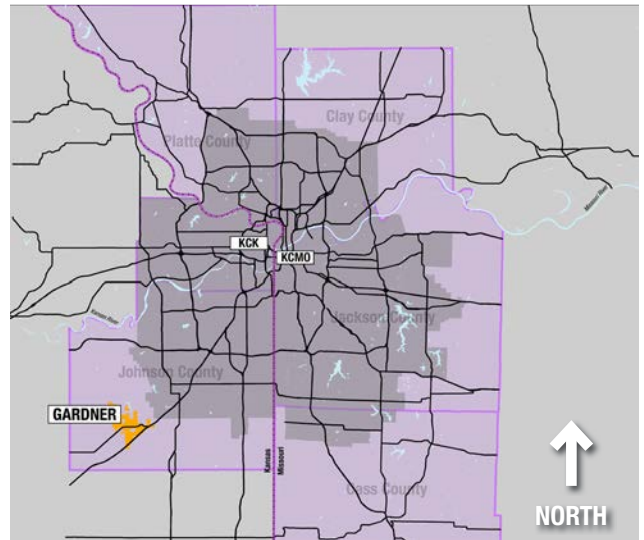


Figure 1.04 - Regional location of Gardner, Kansas



Figure 1.05 - Project Area



## 1.4 KEY ISSUES

The project team performed an inventory and analysis of pedestrian and vehicular circulation and ADA parking issues. Extensive outreach and ongoing involvement with City staff, project champions, property owners and citizens resulted in the identification of several key issues.

### PUBLIC SPACES

Fundamental to this plan is the creation of flexible public spaces in Downtown Gardner that serve as civic gathering venues, something that is currently lacking in the City. Creating these spaces will require several key partnerships between landowners and the City.

### STREETSCAPE

The streets of Downtown Gardner physically knit the community together and act as a connector between the proposed public spaces. Therefore, a second key aspect of this plan is to establish a design vision for the Downtown streetscapes that works to enhance the public realm and creates a revitalized and contemporary streetscape environment.

### ADA FACILITIES / PARKING

Accessibility for everyone in the Downtown core is of the utmost importance to City officials as well as the planning team. A comprehensive study of existing ADA accommodations was conducted, resulting in recommendations for future ADA accommodations that will ensure the proposed improvements can be enjoyed by all users.

Parking was also at the forefront of all design considerations and the planning team made it a goal to not reduce parking numbers in the Downtown core.

The ultimate vision proposes a slight increase in available public and private parking. However, parking is reorganized within the project area to support introduction of new green space and infill development. When the vision of the plan is realized, Downtown Gardner will be a more walkable and pedestrian-friendly district.

### MOBILITY HUB

The final key issue is the establishment of a mobility hub in Downtown Gardner. As Gardner continues to grow as a part of the expanding Kansas City metro, providing access to public transportation is important to assure all citizens have the ability to access all parts of the metro without the need for a personal vehicle. The mobility hub is also designed to accommodate more recent transportation options such as ride sharing, bike sharing and scooters / short distance options.



Figure 1.06 - Example of a Downtown gathering space in Gladstone, MO



Figure 1.07 - Example of traditional streetscape design character



Figure 1.08 - Example of a Mobility Hub used in Kansas City



## 2 EXISTING CONDITIONS



## 2.1 GREEN SPACES

Open air public spaces can host festivals, concerts, meetings and recreational activities and they are an important venue to have in a community as it adds great vitality and pride to the social fabric. Currently, Gardner does not have such a space that serves as the formal “heart” of the City.

**A** Cornerstone Park, located at the corner of Washington Street and Center Street, is currently the only public green space within the Downtown core of Gardner. This ~2 acre park contains playground structures, three picnic shelters, an amphitheater, restrooms, and walking trails. Located adjacent to the Gardner Aquatic Center, Cornerstone Park is most heavily used during the warmer months when the Aquatic Center is open. Despite these assets, the park is not used to its full potential and is somewhat disconnected from the central business district along Main Street.

**B** Located between Washington Street and Shawnee Street, a large underutilized green space, primarily owned by First Presbyterian Church, provides an opportunity for further investment in public spaces. This site is characterized by expansive turf, a few mature trees and powerlines crossing the site east / west. Blazer’s, a popular Gardner eating establishment, is also located directly to the west of this property.

**C** A third opportunity to create a public open space lies directly behind City Hall along E Shawnee Street. As an important civic institution, City Hall in many ways represents the center of Gardner and the space around it should respond to that prominence. Today, the site is primarily a surface parking lot for City Hall and the local Johnson County Library, who owns the majority of the property. Also located on this site is a group home, a small private residence and a commercial building at the Corner of Main and Center Street. An entrance drive from Main Street along the east side of City Hall provides accessible parking for City Hall.



Figure 2.02 - Existing underutilized Cornerstone Park amphitheater



Figure 2.03 - Large underutilized green space between Shawnee and Washington Street



Figure 2.01 - Proposed public space improvements



Figure 2.04 - Existing surface parking lot behind Gardner City Hall and JOCO Library



## 2.2 DOWNTOWN STREETSCAPES

The streetscape of Gardner's Downtown is dominated by vehicular use with ample space for driving and parking cars. Even with substantial sidewalk space along Main and Elm Street, there are very few elements that make the pedestrian feel protected from the busy highway traffic. The Downtown streetscape could provide greater potential for civic engagement and economic vitality.

Main Street, or Highway 56, is the main thoroughfare through Gardner. As the primary east/west route, and connector to larger industrial sites in the west, this road experiences a high degree of everyday traffic and a high quantity of truck traffic. Many of the parallel parking spaces along Main Street are not fully utilized because of the sense that they are unsafe with relatively high speed truck traffic occurring immediately adjacent to that zone.

Elm Street currently retains a more pedestrian-oriented feeling with more intimate sidewalk spaces which are protected from moving traffic by diagonal parking spaces. This design also creates a more successful business climate. Nonetheless, much more can be done with Elm Street to further capitalize on its already established pedestrian quality.

It is important that streetscapes be designed with the pedestrian in mind, as they are the most important user of the space. Even though vehicles will bring people to this space, it is users on foot traveling around the Downtown core that create an active and bustling district. Therefore, elements such as seating, pedestrian lighting, street plantings, wayfinding signage and other amenities are important elements to consider for a streetscape. It is these amenities that signal who the streetscape is designed for and encourage users to come back time and time again. These spaces and features also provide an opportunity to tell Gardner's stories, furthering the unique qualities of Downtown.



Figure 2.05 - Major streets in Downtown Gardner



Figure 2.06 - Elm Street (looking north)



Figure 2.07 - Main Street (looking east)





# 3 PLANNING PROCESS

### 3.1 PROJECT TIMELINE

The project began in May 2019 and extended into the first quarter of 2020. Three main phases helped set attainable goals for public participation and results. Those three phases were Discovery (2.5 months), Ideas (4 months) and Implementation (4months).

The Discovery phase is where the majority of the inventory and analysis process took place. Documenting existing conditions and meeting with government and regulatory officials provided a baseline understanding of future challenges and opportunities for the project.

The Ideas phase brought about concepts for the green spaces and streetscapes with many iterations discussed between the planning team, city leaders and the general public. Overtime those concepts were refined into one final concept.

Finally, the Implementation phase involved the development of this plan and a strategic approach to financial and policy initiatives necessary to make these designs come to fruition.



Figure 3.01 - Steering Committee Workshop

PROJECT SCHEDULE											
	MAY 2019	JUN	JUL	AUG	SEP	OCT	NOV	DEC	JAN 2020	FEB	MAR
DISCOVERY											
IDEAS											
IMPLEMENTATION											
PLANNING COMISSION / CITY COUNCIL											

Figure 3.02 - Project Schedule

### 3.2 MEETING GROUPS

#### STEERING COMMITTEE

The recommendations outlined in this plan were derived from an open and transparent process. The planning team coordinated a series of six meetings with the Steering Committee, largely composed of the same citizen leaders who assisted with the precursor to this plan, the 2018 Main Street Corridor Plan. However, additional citizens were engaged during the implementation exercises for this plan. Each of these meetings provided opportunities for the Steering Committee to better understand the issues being explored as part of the planning process, to ask questions and obtain additional information, and to provide their input and direction for shaping the proposed plan recommendations.

#### PROJECT PARTNERS + STAKEHOLDERS

The planning team coordinated meetings with MARC and KDOT throughout the project. Additional conversations occurred with the Kansas City Area Transportation Authority (KCATA) regarding bus service in the Downtown Corridor and the planned mobility hub.

The planning team also met with multiple key stakeholders that could be directly impacted by and benefit from the project, or who had additional information about the areas envisioned for investment and the Gardner community. This included property and business owners, church officials, the Johnson County Fair Board, and local civic organizations.

These conversations provided the planning team with great insight as well as a platform for partnership and ongoing dialogue as the City moves forward.

### 3.3 PUBLIC ENGAGEMENT / OUTREACH

In addition to the Steering Committee meetings, the process involved many opportunities to engage the public.

#### BENCHMARKING BUS TOUR

The first of these meetings took place on a RideKC Bus as participants toured various public spaces and streetscapes throughout the Kansas City metro. The intent of this engagement was to allow residents to see firsthand how other communities have rallied around the process of developing a public community asset and actually implemented their visions.

The locations visited included:

- Overland Park Farmers Market - Overland Park, KS
- Linden Square - Gladstone, MO
- Downtown Liberty, MO
- Uptown Market - Independence, MO
- Downtown Lee's Summit - Lee's Summit, MO
- Park Place - Leawood, KS

These locations exemplified different types of public spaces but all share one thing in common: they have made a difference by bringing about the opportunity for people to gather together and be proud of their community.

Overland Park demonstrated how a formal farmers market structure, combined with parking, can help activate the Downtown frequently.

Linden Square, a public green space and amphitheater in Downtown Gladstone, conveyed the idea of how different property owners and city officials, as well as private developers, were able to work together to implement a successful performance venue that catalyzed private development.

Downtown Liberty recently invested in a refreshed streetscape environment, and in the context of their historic Downtown, the streetscape brings contemporary touches with an overall layer of historic qualities. The streetscape design has proven to be popular with the public and only enhances their already vibrant Downtown.

Uptown Market in Independence is a market / community building structure that was recently constructed adjacent to the Downtown square of Independence. Having a building that functions for both market activities as well as festival and community events brings frequent activation to Downtown Independence.

Downtown Lee's Summit is a great example of a cohesive Downtown streetscape that blends the texture of the historic buildings and pedestrian oriented design elements. Business signage, wayfinding elements, street plantings and paving details all combine to create a warm and inviting environment.

Park Place in Leawood represented a slightly different model for the bus tour group to experience. Brought about by a private developer, Park Place is filled with higher end stores and restaurants but at its core is a green space meant for activities and passive recreation. This space is successful because of the right amount of nearby mixed-use development and the activity that it provides.



Figure 3.03 - Bus Tour Group



There were several other opportunities for the public to offer their ideas to the planning team throughout the project. Those opportunities engaging with folks at a Food Truck Frenzy + Farmer's Market in Downtown Gardner, and individual meetings with stakeholders.

In addition to those events, two online surveys were administered and distributed through various social media outlets as well as the City's website. Providing a way for Gardner residents to give feedback when it is convenient for them garners higher levels of response. In fact, the first survey had 536 responses which equals 2.5% of the entire population of Gardner. The final proposals include the preferred amenities, programmatic elements and space flexibility that the public indicated through the survey. This feedback was of value to the planning team in crafting a vision that satisfied the community at large. The complete survey results can be found in the Appendix.

Finally, over the course of the project, two public meetings were held at the Gardner Community Center. Public Meetings / Open Houses allow for citizens to come and speak directly to the planning team and city officials, better understand the project and share any direct concerns or approvals.

## PUBLIC FEEDBACK

Overall, the vision for Downtown Gardner was well received and generated a lot of excitement for what it holds for the future of the City. Concerns and challenges were voiced by the public and heard by the planning team, and over the course of the project, the initial concepts were fine tuned to address those issues. In the end, it is the belief of the planning team, the steering committee, and city officials that the vision laid out in this plan will bring positive investment and create a true heart in the City of Gardner.



Figure 3.04 - Photos from various engagement meetings / events



## 3.4 WALK & ROLL INVENTORY

On July 25th, 2019, the project team conducted a walk & roll inventory in Downtown Gardner to identify access improvement opportunities for pedestrians and mobility-impaired persons. The project team consisted of members of the Steering + Implementation Committee, city staff, teaming partner Wilson & Company, and members of the public who utilize mobility aids. The team was tasked with identifying areas where ease of access could be improved. The team primarily focused on sidewalks, curb ramps, crosswalks, building entrances, and the location of ADA parking in the Downtown study area.

The following areas were identified as needing major improvement:

- Access to front building entrances along Main Street (private property).
- Sidewalk pavement surface.
- Curb ramps surface.
- Crossing distance at crosswalks.
- Crosswalk surface.
- ADA stall distance to priority buildings.

Specific action items identified from the inventory include:

- Addressing building entrance slope and width along Main Street to allow for full ADA access.
- Interim solution: Encourage businesses on Main St. who have existing ADA access in the rear of the store with alley access to clearly indicate where and how access the building entrance (example – doorbell and a sign).
- Replace sidewalk on the south side of Park St. between Center St. and Sycamore St.
- Add sidewalk to the north side of E Washington St. east of Sycamore St.
- Investigate alternatives to the current sidewalk repair program to address thin sidewalk patching that causes debris.
- Encourage private property owners to maintain their section of sidewalk.
- Add additional on-street ADA stalls throughout Downtown, specifically van stalls with a passenger access aisle and an ADA stall in front of City Hall.
- Enhance bump-outs to all corners of Main St. and Elm St. to decrease crossing distance.
- Add a pedestrian refuge/median-cut on Main St. where pedestrians cross along Elm St.
- Asphalt repairs at all pedestrian crossings along Elm St.
- Installation of a sign at Main and Center to warn drivers of pedestrian crossing.



Figure 3.05 - Examples of accessibility challenges in Downtown Gardner





# 4 CONCEPTS

## 4.1 PUBLIC SPACES - FINAL CONCEPT

Three initial concepts were developed to garner feedback about larger design ideas as well as programmatic elements, vehicular and pedestrian circulation and general aesthetics. The greatest difference in these plans was based on concepts for the property between Washington Street and Shawnee Street. Majority owned by the adjacent First Presbyterian Church, these initial concepts explored different scenarios of how much land would be available for redevelopment. These scenarios can be found in the Appendix.

In the final concept, the vision of a strong visual and physical connection between Main Street and Cornerstone Park remains intact with many of the important features of this concept relating to that overall axis. Located directly adjacent to the City Hall on the east, a pedestrian plaza with a iconic vertical monument anchors the entire spine of public space to Main Street. The vertical monument serves two purposes; it becomes an identifiable feature in the Downtown core that draws attention to the new public spaces, and it has the opportunity to speak to the history of Gardner in either an artful or a documental way. For example, this would be a nice opportunity for a mural on the blank wall at the east entrance to the plaza adjacent to City Hall.

Within City Hall Plaza, parking, gathering spaces and a public market all work together to create a truly dynamic and active space. Because of added hardscape surfaces, stormwater capture is an important aspect for implementation to ensure this project is as sustainable as possible. The public market will now have a permanent structure. Depending on the

size of the market in the future, nearby parking stalls can also be used for vendors and merchants.

Extending across Shawnee Street, a large road table is proposed to ease pedestrian access from one space to the other and encourage lower vehicle speeds. The path leads directly into the amphitheater lawn and seating area. This space is designed to be flexible and host events of varying sizes. A small overhead structure acts as the performance space and the adjacent plaza contains a pergola structure for more cover and protection. Immediately behind these structures, a second seating area serves as an option for patrons of Blazer's which is adjacent to this public space.

Road tables once again facilitate pedestrians crossing from Washington Street into Cornerstone Park. A multi-purpose building is proposed for the western end of the park, along Center Street, adjacent to the park lawn. Designed to be unprogrammed and porous, the multi-purpose building received favorable ratings during public meetings. At the terminus of one of two main pedestrian paths, a prominent water feature is intended to be both sculptural and interactive and will appeal to families and visitors. A destination playground and picnic shelters nearby facilitate even more activities in Cornerstone Park.

Residential units are proposed along Main Street in a financially viable quantity. Furthermore, mixed-use developments at the corner of Center Street and Main Street, and along the south side of Main Street, help to fill in gaps of the corridor's business district and also increase the amount of commercial and residential tenants.



**Figure 4.01** - Plan of proposed public space and streetscape improvements in Downtown Gardner. A full page view of this plan graphic can be found on page 56 in the Appendix.



- A** Directly behind City Hall, the Gardner Public Market would utilize a permanent overhead structure to increase attendance and ensure its long-term success. Located along the axis of the rear door of City Hall, the structure will anchor a larger plaza space intended to be used for community events of all sizes. While the market and events are not occurring, this area will satisfy the parking needs of City Hall and other nearby businesses. Every opportunity should be made to implement stormwater infrastructure around this plaza space so that it positively contributes to Gardner's environmental sustainability goals.
- B** West of City Hall, a public promenade alongside new redevelopment offers the opportunity for Downtown employees and visitors to meet up for lunch or other social outings. The new development at the corner of Center and Main could house a restaurant or retail with office or residences on the upper floor. Gardner could commission a mural for the west facade of City Hall, making this space even more dynamic.
- C** Looking Northeast, the full scale of the recommended public spaces improvements in Gardner can be viewed. New Mixed-Use and residential developments along Center Street frame the western edge of the Public Market, Amphitheater Lawn and Botanical Gardens. Plaza spaces alongside City Hall extend these public spaces to the edge of Main Street and create one unified hub of activity in the center of Gardner.



Figure 4.02 - Gardner Public Market permanent facility directly behind City Hall



Figure 4.03 - Public promenade west of City Hall



Figure 4.04 - Aerial view of proposed public spaces with the Public Market structure and new mixed-use and residential developments shown in the foreground



- D** The plaza adjacent to City Hall on the east side accomplishes several goals. It extends the public spaces recommended behind City Hall to the edge of Main Street and creates visibility for the entire system of spaces. A proposed ADA accessible parallel parking stall on Main Street, directly in front of this plaza, eases access into City Hall because the handicap accessible ramp is accessed from this corner of the building. This stall should be implemented early in project phasing. The vertical monument is an excellent opportunity to speak to the history of Gardner through informational and artistic components.
- E** Located between two parking areas, and along the axis that connects all of the public spaces together, is a small pedestrian plaza with stormwater retention gardens and a covered seating / events area. The surrounding parking areas are recommended to be at the same elevation as the small plaza with the intent that events of varying sizes can be hosted here and extend as far as they need to while also accommodating all ranges of accessibility.
- F** Looking northwest, the pedestrian plaza located alongside City Hall to the east, with an iconic vertical element, is prominent in the foreground of this view. Leading to the North, a strong visual and physical connection is made through the large plaza space, amphitheater lawn and Cornerstone Park. Hardscape materials in the foreground reflect those that make the entire space feel pedestrian focused, although vehicles will still utilize parking lots and access drives.



Figure 4.05 - Public plaza along Main Street with iconic vertical monument



Figure 4.06 - Eye-level view of green infrastructure elements located in City Hall Plaza



Figure 4.07 - Aerial view (looking northwest) showing all three public spaces connected by diagonal walking path



**G** This view shows a close-up view of the amphitheater lawn and adjacent hardscape plaza space. Large curved seat walls provide seating while also allowing the space to be flexible. It is intended that turf and hardscaped areas function as one space when larger events require that amount of space but that these spaces can also function separately for smaller events or day-to-day passive recreation. The nearby residential units will have prime viewing of these green spaces which will add to their market appeal.

**H** Looking south down one of the two main pedestrian paths, hardscape areas behind the amphitheater lawn and performance venue provide opportunities for large and small gatherings. Overhead shade structures help protect visitors from direct sunlight. The seating area to the right in this view is directly behind Blazer's restaurant and could serve as a convenient and pleasant outdoor patio for diners.

**I** Looking north from Shawnee Street, this view depicts the botanical garden in the foreground, which envelopes the amphitheater space and lawn. This garden could be a native exploration garden that retains stormwater runoff and therefore acts as an educational and sustainable amenity. Mounding on the ground plane provides clear sight lines for events, and the botanical garden, with its meandering paths, creates a nice buffer between the street and residential units from the more active lawn space. Removable bollards across Shawnee Street are recommended for larger events making City Hall Plaza and the Civic Green one large public space.



Figure 4.08 - Amphitheater Lawn (looking north) to be used as flexible open space



Figure 4.09 - Hardscape areas designed for small and large gatherings / festivals



Figure 4.10 - Aerial view of Botanical Garden, Amphitheater Lawn, Event Space and new residential development, north of Shawnee Street



**J** Viewed up close, the improved playground and interactive water feature together will create a place where children and families will want to spend time and hang out, which in turn will activate the entire park space. Walking paths also allow for leisurely exercising and other passive activities.

**K** A prominent feature of the redesigned Cornerstone Park is the proposed Multi-Purpose Community Building. Designed to be versatile, community events of all sizes can take place here. Garage doors on the facade allow for events to spill over into the park lawn if necessary. This facility would be a great asset to Gardner and provide regular activation to Cornerstone Park.

**L** Cornerstone Park is an important public space in Gardner but could be improved to maximize its full potential. In this view, the improvements can be seen. The existing playground is recommended to be modernized; park pavilions remain with some minor updates. The biggest change is a proposed multi-purpose community building that overlooks a park lawn. These two elements are intended to be very flexible and be utilized for events of varying sizes throughout the entire year. An interactive water feature, located in the middle of the Park and at the end of the axial connection with City Hall, becomes another iconic and playful element in Gardner. Parking capacity at the adjacent Aquatic Center can be used for events in Cornerstone Park and/or shuttles can be utilized from farther locations depending on event sizes. Coordination will be needed to make the necessary agreements.



Figure 4.11 - Playground and interactive water feature in Cornerstone Park



Figure 4.12 - Cornerstone Park Lawn designed as a flexible multi-use space



Figure 4.13 - Birds-eye view of Cornerstone Park (Looking southwest)



## 4.2 DOWNTOWN STREETSCAPES - FINAL CONCEPT

### STREETSCAPE - DESIGN FRAMEWORK

During the 2018 Gardner Main Street Corridor Plan, it was established that community members preferred a more traditional aesthetic when it came to streetscape elements. It was agreed upon that a traditional theme would fit nicely with the character of Downtown.

For the purposes of this study, we further developed design concepts for the streetscapes and a 'kit of parts' for streetscape elements based off the original feedback given in 2018.

More information on the initial concepts can be found in the Appendix.

In order to develop a design strategy for the streetscapes in Downtown Gardner, the streets were classified into three different categories; Primary, Secondary and Tertiary.

Primary Streets typically have the most traffic, commercial and civic activity associated with them. Main Street was the historic route through Gardner and is still a major east/west arterial in its current status as State Highway 56. Street trees along Main Street relate to

the spacing of the parallel parking stalls and are placed every 40'. Elm Street is also classified as a Primary Street because together with Main Street, it forms the 4-corners of the Downtown District. Street trees along Elm Street are spaced every 25'. Additionally, Elm has many small businesses along it and connects important assets such as the Johnson County Fairgrounds, Johnson County Library, and Gardner Elementary School.

Secondary Streets are important connectors within the Downtown District and form the basic street grid pattern. Secondary Streets function to circulate vehicular and pedestrian activity throughout the Downtown District but always in relation to the Primary Streets. Street tree spacing varies on secondary streets as they relate to the configuration of the on-street parking (parallel vs. angled).

Tertiary Streets typically have less activity and traffic and in the context of Gardner, are primarily made of residential land uses. Street tree spacing along Tertiary Streets is recommended to be 50'.

See Figure 4.14 to see the streetscape classifications.

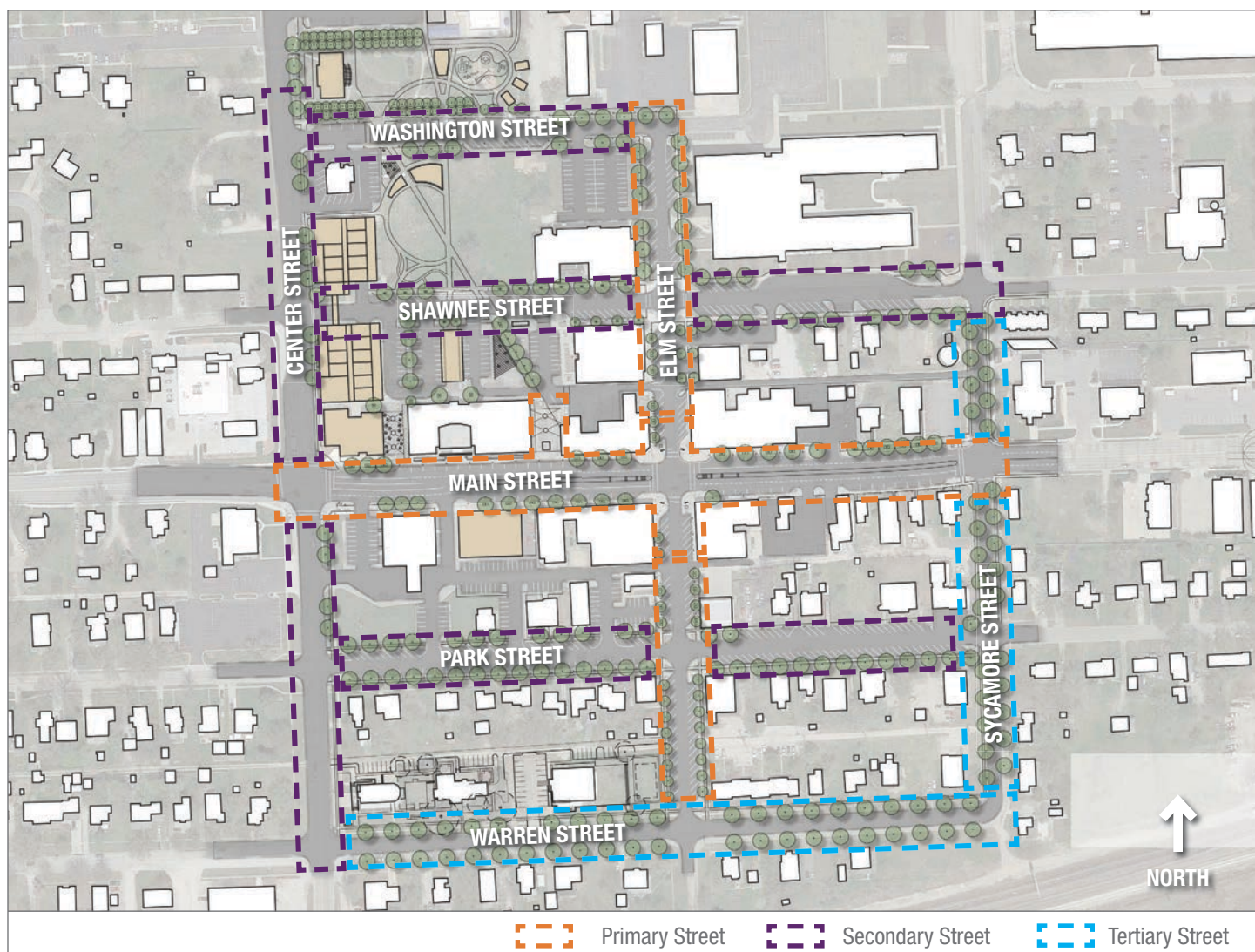


Figure 4.14 - Streetscape categories in Downtown Gardner

STREETSCAPE - SECTIONS

This hierarchy of streets is important when considering streetscape improvements. Not all streets can receive the same level of investment, nor should they. Streetscape improvements include paving materials, bench seating, landscaping, etc. Because of their higher use and prominence, Primary Streets will receive the highest level of investment, followed by Secondary Streets and Tertiary Streets.

For this study, the streetscape categories indicate the following streetscape elements:

Primary Streets

- Street Trees
- Decorative Pedestrian Lighting
- Site Furnishings (bench seating, trash receptacle, bike rack, etc.)
- Decorative Pavement
- Wayfinding Signage
- Light Pole Banners
- Festival Lighting
- Gateway Markers

Secondary Streets

- Street Trees
- Minimal Site Furnishings (bench seating, trash receptacle, etc.)
- Minimal Enhanced Landscape

Tertiary Streets

- Street Trees
- Minimal Enhanced Landscape

Figure 4.15 illustrates conceptual streetscape elements for Downtown Gardner. Not all elements listed above are depicted such as decorative pavement and festival lighting. The design of these elements can vary greatly and will need to be a considered before implementation.

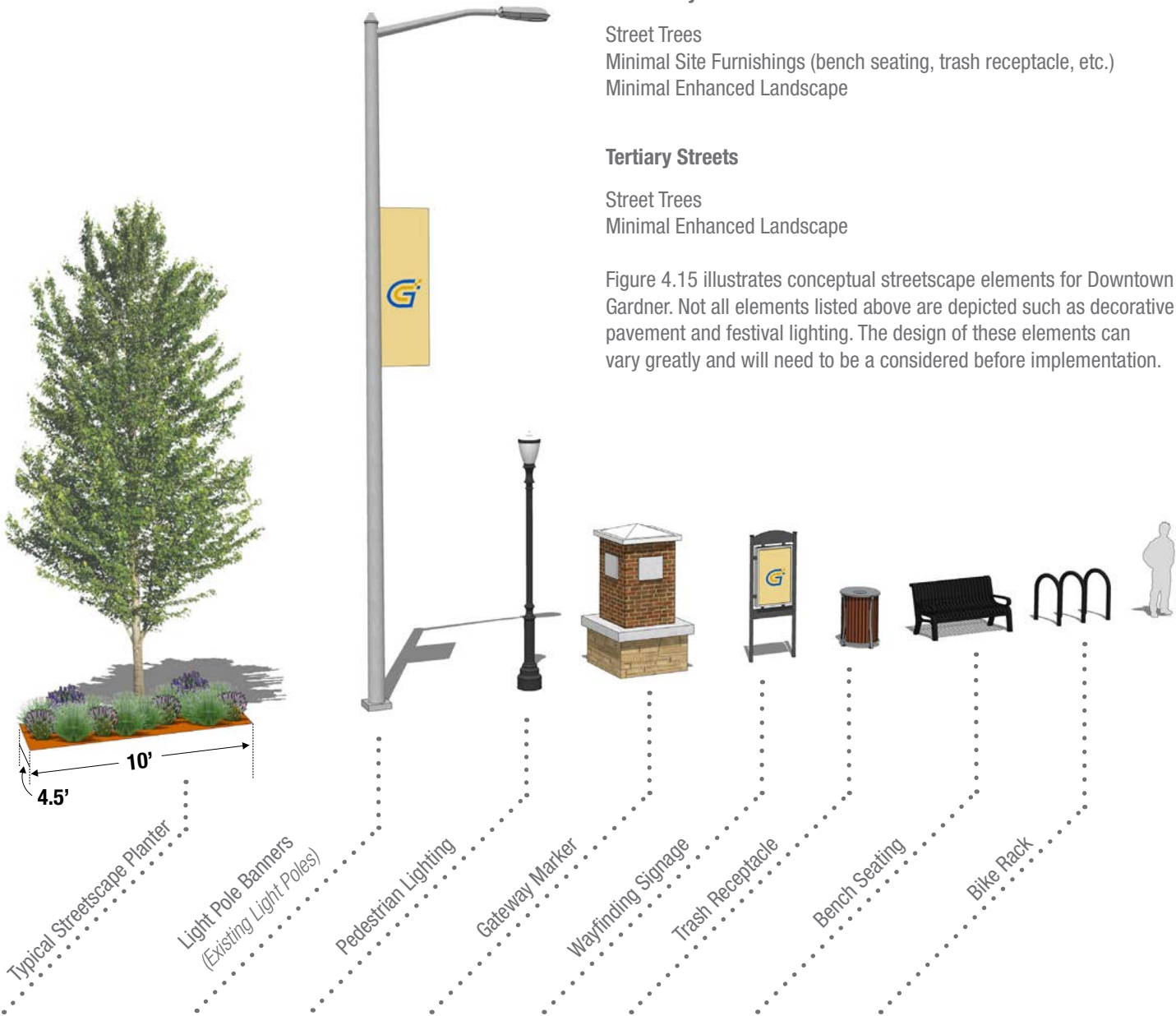


Figure 4.15 - Conceptual Downtown streetscape elements



## STREETSCAPE - SECTIONS

Figures 4.16 - 4.19 illustrate the typical roadway section for each of the street categories: Primary, Secondary and Tertiary. Sections are depicted for both Main and Elm Streets because they have a very different feel from one another even though they are both categorized as Primary Streets.

The recommended conditions for Main Street (Figure 4.16) are a direct result from the work done in the 2018 Main Street Corridor Plan. Curb locations and four travel lanes remain to accommodate high levels of car and truck traffic, but lanes are reduced from 12' to 11' to encourage slower speeds and allow for a center median. The median, with brick pavers and limestone blocks, serves to slow traffic and enhance the overall streetscape of the Downtown core. 10' parallel parking lanes are maintained but now include the 1.5' gutter, which previously was not the case. The ample sidewalks remain unchanged in size but are recommended to have an amenity zone within its footprint. The amenity zone should contain street trees, furnishings, wayfinding signage and pedestrian lighting. Decorative pavers throughout Main Street will enhance the character of the street and make an enjoyable pedestrian experience.

Elm Street is smaller scale and more pedestrian focused than Main Street (Figure 4.17). Travel lanes on Elm are reduced to 11' and diagonal parking is increased by 2' on each side. This realignment allows an increase in sidewalk width on both sides of the street. Paving patterns and materials carry over from Main Street with street trees and pedestrian lighting in the amenity zone providing comfort to pedestrians. Because of spatial limitations, not as many streetscape elements can be accommodated but where possible, amenities should be included.

Shawnee Street is a Secondary Street and Figure 4.18 illustrates the general character. Lane widths of 14' work within the existing curb line and diagonal parking is located on the North side of the road. As shown in the distance, a road table allows easy pedestrian access across Shawnee Street and encourages motorists to slow down. Streetscape elements include street trees, minimal site furnishings and minimal enhanced landscape features within the amenity zone.

Figure 4.19 shows a section of Sycamore Street, which represents a Tertiary Street. Most existing conditions would remain for this category. Street trees should be added at regular intervals and minimal enhanced landscape elements, such as small beds of grasses or perennials near intersections, should be added where possible. On-street parking will remain unchanged.

Street trees are recommended to be deciduous and columnar in form with hardy perennial foundational plantings where applicable. Plants should reflect a mixture of colors for seasonal interest. Plants should have reduced heights near intersections to keep sight lines intact. Final plant spacings and selection will be defined during the design process.

Collectively, the categories of streetscape design establish a hierarchy of design in Downtown Gardner and convey to visitors and residents that Main and Elm represent the heart of the City while also enhancing all streets to be a safe and comfortable pedestrian experience.

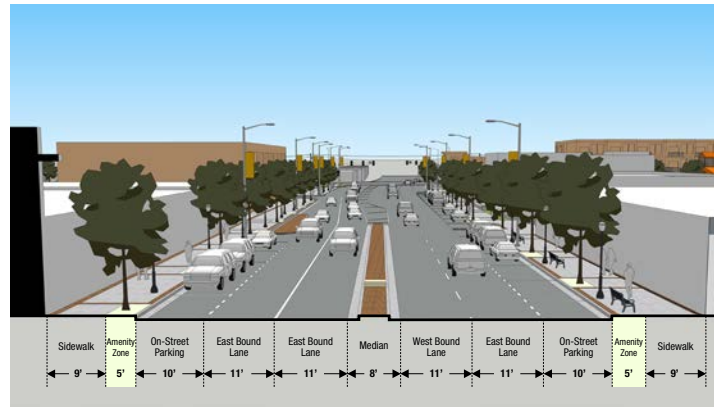


Figure 4.16 - Section, looking west, on Main Street

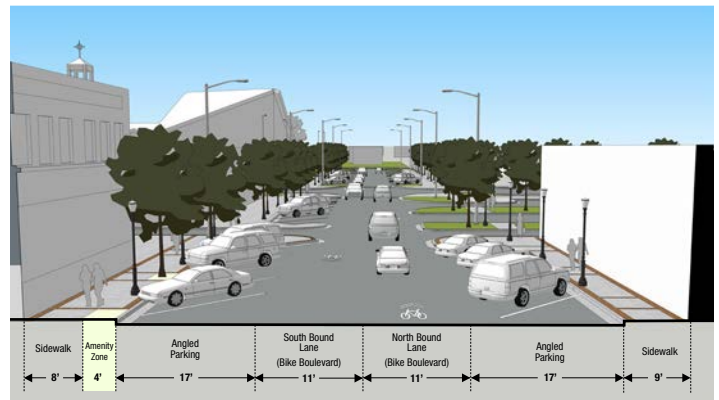


Figure 4.17 - Section, looking north, on Elm Street



Figure 4.18 - Section, looking east, on Shawnee Street

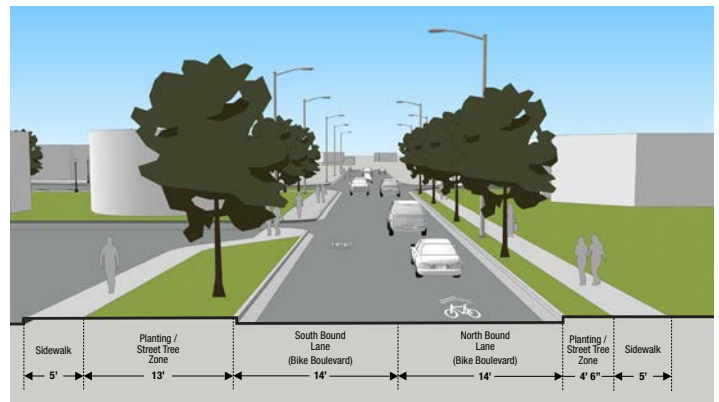


Figure 4.19 - Section, looking north, on Sycamore Street



## STREETSCAPE - FINAL CONCEPT

- A** Creating refuge moments for pedestrians along the streetscape was an important consideration in this design. An amenity zone along the curb line provides space for streetscape elements such as pedestrian lighting, seating, wayfinding signage and other site furnishings. Additionally, bump-outs at the intersections offer more space for landscape beds, gateway markers and bench seating while also acting as a traffic calming element.
- B** Looking east on Main Street, the proposed ADA van accessible stall can be seen in the foreground, adjacent to the pedestrian plaza and vertical monument. Paving details can be seen in this view with brick banding creating a nice rhythm on the ground plane and giving the streetscape a warm touch.
- C** The cornerstone of Gardner's streetscape is the Main Street and Elm Street intersection. As the historic center of Gardner, this intersection represents the heart of the City. The recommended streetscape improvements are clearly visible in this aerial view. Brick paver details create a warm and inviting environment for pedestrians, while the amenity zone and the curbed bump-out areas soften the entire streetscape with planting beds.



Figure 4.21 - Eye-level view of Main Street streetscape with enhanced features



Figure 4.22 - Streetscape improvements along Main Street; proposed ADA parking stall



Figure 4.20 - Birds-eye view of Main and Elm intersection showing streetscape redesign



## 4.3 MOBILITY HUB

A major impetus for this study was to develop a concept for a mobility hub in Downtown Gardner. Communities continue to realize the importance of easy access to public transportation, and it is the intent of developing a mobility hub in the Downtown core that public transportation will be even more accessible and dependable for more members of the community.

Following extensive coordination with the KCATA, the mobility hub is planned on Park Street at the corner of Center Street. RideKC buses will travel west along Main Street (Highway 56) and turn South on Sycamore Street. From there, the buses will turn right (west) on Park Street.

After stopping at the mobility hub, the buses will turn north on Center Street and finally back east on Main Street towards Interstate 35 and Kansas City. This route will offer streamlined service and will not greatly impact day-to-day vehicular traffic.

The mobility hub itself is proposed to include a covered shelter for bus patrons to be protected from the elements. The shelter includes an electronic messaging board to alert riders of the bus schedule and provides the opportunity for advertising and wayfinding elements as well. Adjacent to the bus shelter, racks for bicycles (personal bicycles and bike share) as well as scooter shares will make this mobility hub work for several different modes of transportation and users of all ages. If desired, this location can also function as a formal ride sharing pick-up and drop off location for all of Downtown Gardner.

### BIKE BOULEVARDS

Based on the final recommendations of the Gardner Main Street Corridor Plan, bike boulevards should be added to Washington, Shawnee, Elm, Sycamore and Warren Street. Bike boulevards are preferred bicycle routes that parallel major arterial roads to ensure safer travel conditions for cyclists. This type of facility requires minimal street markings and signage. The implementation of the boulevards to the applicable streets is included in the anticipated streetscape improvements and the costs are reflected in the final streetscape costs, found in the Implementation chapter of this report.



Figure 4.24 - Mobility hub at Center and Park

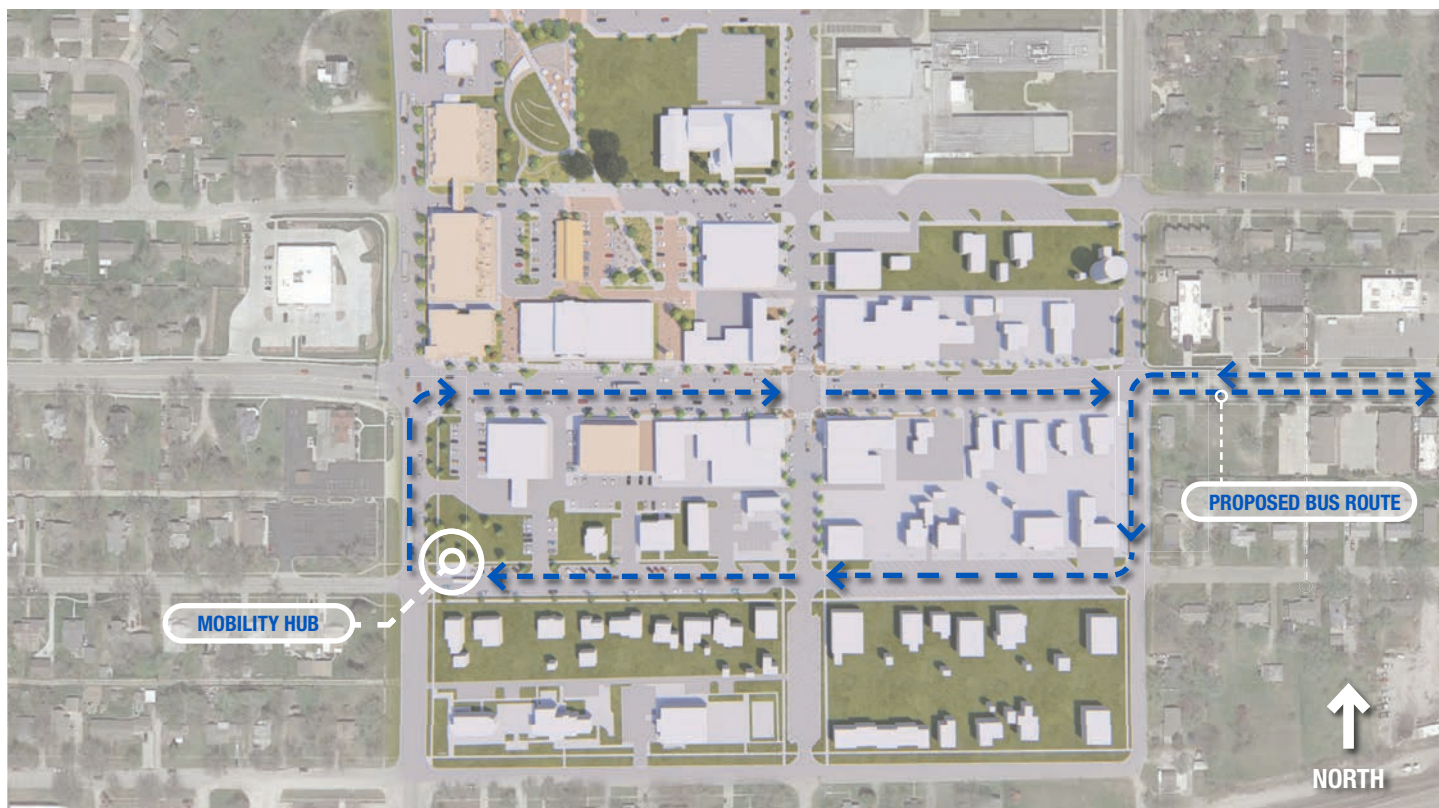


Figure 4.23 - Proposed Mobility Hub location and bus route

## 4.4 ADA FACILITIES / PARKING

The location and availability of parking is an important factor in the success of Downtown Gardner as a walkable and sustainable place. The location of parking has been identified as a concern through various public involvement efforts. The proposed solution strives to maintain existing parking and, possibly increase parking, but this is dependent on final redevelopment solutions in the future. The location and number of accessible stalls was of particular concern and analysis. Regulations established in the Americans with Disabilities Act (ADA) require that those with disabilities have equitable access to their destinations. In particular, Section X02.6.1 of the ADA regulations require a minimum number, location, and slope of accessible spaces. The Walk & Roll inventory conducted through this planning effort further examined the location and access to existing ADA stalls in the Downtown study area.

Several of the parking related items noted in the Walk & Roll inventory process included:

- Lack of on-street ADA parking spaces.
- Proximity of on-street ADA parking spaces to business entrances.
- Non-compliant cross-slope and quality of ADA stall pavement (Elm St).
- Lack of 36" of clear path from ADA stall surface to sidewalk.

Note that these findings were further validated and found to be important factors in ADA regulations.

Parking in Downtown Gardner can be public (such as the stalls behind City Hall) or private (Blazers Restaurant parking lot). Both private and public ADA parking must be considered in order for the Downtown area to provide equitable parking and access. While Gardner has an existing planning process for the review of new developments to include plans for ADA stalls, guidance for the placement of on-street ADA parking on existing streets is less clear.

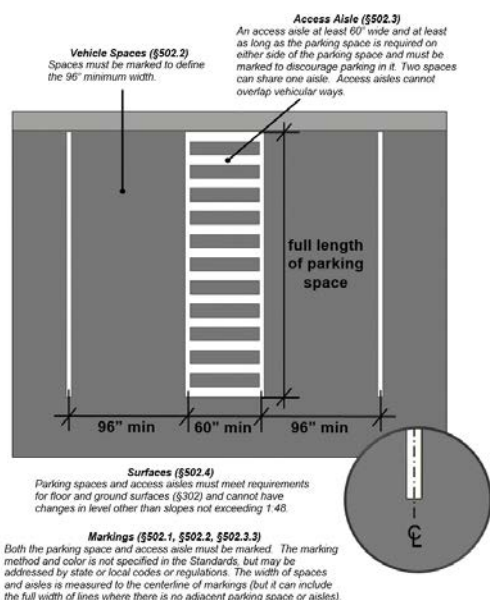


Figure 4.25 - Regional location of Gardner, Kansas

On-street parking is a valued public parking resource for those traveling into the Downtown area by car as it often provides priority access to a storefront. A review of existing regulations as pertaining to on-street parking revealed the following as important considerations when determining the type and placement of ADA stalls:

- 1 van stall needed for every 6 standard accessible spaces.
- 100 or more parking spaces require 1 van / 3 standard stalls.
- Slope should not exceed 1:48 in any direction.
- On-Street ADA van stalls are possible – 5' of clearance on the passenger side and 5' aisle needed.

Additionally, where on-street parking is provided, a required minimum number of accessible on-street parking spaces is defined by the project or project area and in accordance with ADAAG Section 208.2 (see table). Note that stalls need to be dispersed throughout the project area and provide equal access. The project area defines the number of stalls and if equal or greater access is provided, minimums can be calculated within the project area extending into adjacent parking lots or other streets.

PARKING FACILITY TOTAL	MINIMUM NUMBER OF ACCESSIBLE SPACES		
	STANDARD	VAN*	TOTAL (STANDARD + VAN)
1-25	0	1	1
26-50	1	1	2
51-75	2	1	3
76-100	3	1	4
101-150	4	1	5
151-200	5	1	6
201-300	5	2	7
301-400	6	2	8
401-500	7	2	9
551-600	9	2	11**
601-650	10	2	12**
651-700	10	3	13**
701-750	11	3	14**
751-800	12	3	15**
801-850	13	3	16**
851-900	14	3	17**
901-950	15	3	18**
951-1000	15	4	19**
1001-1100	16	4	20**
1101-1200	17	4	21***
1201-1300	18	4	22***
1301-1400	19	4	23***
1401-1500	20	4	24***
1501-1600	20	5	25***
1601-1700	21	5	26***
1701-1800	22	5	27***
1801-1900	23	5	28***
1901-2000	24	5	29***
2001 and over	25	5	30***

\* at least 1 of every 6 accessible spaces or fraction of 6

\*\* 501 - 1000: 2% of total

\*\*\* 1001 and over: 20 + 1 for each 100 or fraction thereof over 1000

Figure 4.26 - Minimum number of accessible parking spaces



A planning overlay was developed to illustrate the quantity and type of ADA stalls needed to provide equal access for those who utilize parking in the Downtown Gardner. Figure 4.27 displays the total number of standard and van stalls recommended for each block.

Numbers represent the final count of stalls needed per segment. Note that project areas can be combined to alter the minimum number of parking spaces, but only if equal access is provided. “Project Area” was defined as any segment of on-street parking between intersections and any additional parking lot in the proposed plan.

Figure 4.28 shows an ADA approved design to provide standard or van stalls in an on-street scenario. This would provide easier access to the front of City Hall – currently the only ADA accessible entrance. KDOT wouldn’t view this proposed ADA stall differently than the parking that is currently along Main Street.

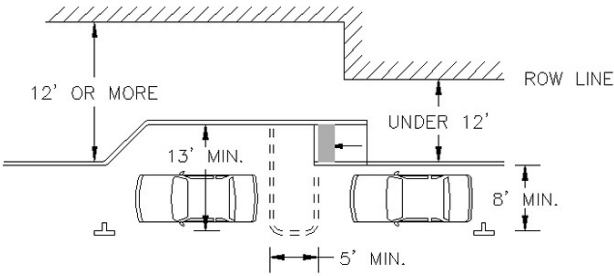


Figure 4.28 - On-Street ADA approved Van accessible stall



Figure 4.27 - Required standard and van ADA accessible stalls for streets and parking lots in Downtown Gardner



# 5 IMPLEMENTATION



## 5.1 IMPLEMENTATION OPTIONS

The following section outlines the projects that comprise the Gardner Destination Downtown vision and details the primary components of each project and the cost associated with those improvements. Also shown are potential funding sources and participating entities responsible for implementing the improvements. A more detailed matrix for the public spaces and streetscape improvements can be found on the following pages.

### PUBLIC SPACES

#### 1 CORNERSTONE PARK

##### 1A. Multi-Purpose Community Building

*Approximately 3,400 SF, The size of the multi-purpose community building can vary to accommodate final program.*

##### 1B. Park Improvements

*Pathway realignment, revamped inclusive playground, interactive water feature, enhanced landscape, park pavilions.*

#### 2 CIVIC GREEN

##### 2A. Property Acquisition

*Area TBD.*

##### 2B. Park Construction

*Pathway construction, amphitheater performance structure, overhead trellises, hardscape plazas, botanical gardens, site furnishings.*

#### 3 CITY HALL PLAZA

##### 3A. Property Acquisition

*Area TBD.*

##### 3B. Plaza Construction

*Parking lot construction, green infrastructure, hardscape plazas, enhanced landscaping, site furnishings.*

##### 3C. Public Market Shed

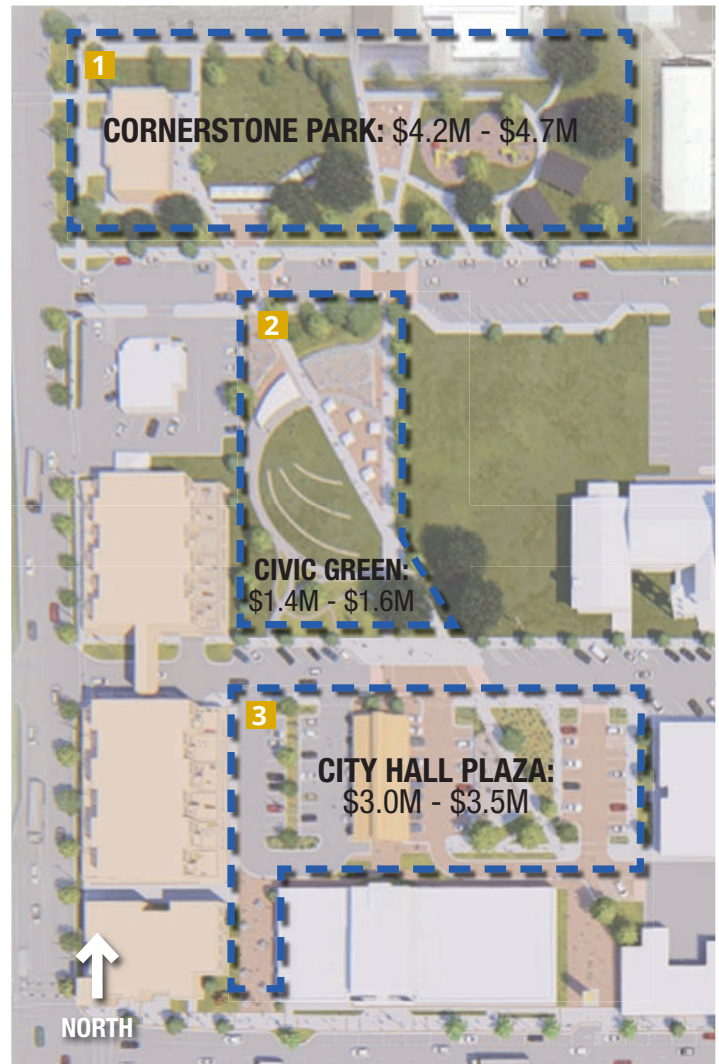


Figure 5.01 - Public Spaces estimated costs



Figure 5.02 - Proposed City Hall Plaza, looking south

PUBLIC SPACES				
PROJECT		EST COST*	POTENTIAL FUNDING SOURCES**	PARTICIPANTS
<b>CORNERSTONE PARK</b>				
<b>1A</b>	Multi-Purpose Community Building	\$1.7 - \$2.0 M		City or Private
<b>1B</b>	Park Improvements	\$2.5 - \$2.7 M	ED, AARP	City
<b>CIVIC GREEN</b>				
<b>2A</b>	Property Acquisition	TBD		Public / Private Partnership
<b>2B</b>	Park Construction	\$1.4 - \$1.6 M	ED	City or Public / Private Partnership
<b>CITY HALL PLAZA</b>				
<b>3A</b>	Property Acquisition	TBD		City or Public / Private Partnership
<b>3B</b>	Plaza Construction	\$2.2 - \$2.6 M	ED	City / JoCo Library
<b>3C</b>	Public Market Shed	\$850K - \$1.1 M	ED	City

**Figure 5.03 - Public Spaces Implementation Matrix**

\*Estimated costs are based on 2019 values and should be updated routinely to account for inflation and other economic factors.

\*\* For definitions of potential funding sources, see Figure 5.04, below.

IMPLEMENTATION KEY			
TA	Transportation Alternatives: Transportation Enhancement	Construction, planning, and design of on-road and off-road trail facilities for pedestrians, bicyclists, including sidewalks, bicycle infrastructure, pedestrian and bicycle signals.	80% Federal / 20% Local (20% minimum)
ED	Economic Development	Funds transportation improvements to recruit new businesses and encourage growth of existing businesses.	25% minimum local match preferred
AM	Access Management	Promotes safe, efficient traffic flow on the Kansas State Highway system while allowing for reasonable access to adjacent properties. This program includes access planning and construction components.	Access Planning - 33.3% minimum local match preferred.  Construction Program - Construction phase of selected projects at 100%, up to a maximum of \$2,000,000. Other associated costs are not eligible for reimbursement.
CCLIP	City Connecting Link Improvement Program - Surface Preservation	Intended to address deficiencies in or extend the life of the driving surface.	Minimum local match 0 - 25% depending on the size of the City.  Maximum state share for Surface Preservation: \$300,000.

**Figure 5.04 - Implementation matrix key terms**

## STREETSCAPES

### 1 MAIN STREET

Street improvements (Center to Sycamore). Where necessary, Main Street is recommended to be reconstructed and raised to alleviate ADA accessibility issues into Main Street businesses and address current sidewalk / ramp conditions that do not meet ADA standards.

*\*Installation of Primary Street streetscape elements.*

*Installation of center median.*

*Construction of vertical monument east of City Hall.*

### 2 ELM STREET

**2A.** Street improvements (Washington to Main). Where necessary, Elm Street is recommended to be reconstructed and raised to alleviate ADA accessibility issues where possible.

*Reconfigure widths of driving lanes, diagonal parking and sidewalks. Application of bike boulevard street markings and signage.*

*Installation of Primary Street streetscape elements.*

**2B.** Street Improvements (Main to Warren). Improved sidewalks and installation of Primary Street streetscape elements. Application of bike boulevard street markings and signage.

### 3 PARK STREET

**3A.** Mobility Hub.

**3B.** Street improvements (Center to Elm). Improved sidewalks and installation of Secondary Street streetscape elements.

**3C.** Street improvements (Elm to Sycamore). Improved sidewalks and installation of Secondary Street streetscape elements.

### 4 SHAWNEE STREET

**4A.** Street improvements (Center to Elm). Improved sidewalks and installation of Secondary Street streetscape elements. Installation of road tables at pedestrian crossings. Additional diagonal parking spaces along north side of street. Application of bike boulevard street markings and signage.

**4B.** Street improvements (Elm to Sycamore). Improved sidewalks and installation of Secondary Street streetscape elements. Application of bike boulevard street markings and signage.

### 5 WASHINGTON STREET

Street improvements (Center to Sycamore). Improved sidewalks and installation of Secondary Street streetscape elements. Installation of road tables at pedestrian crossings. Additional diagonal parking spaces along south side of street. Application of bike boulevard street markings and signage.

### 6 CENTER STREET

**6A.** Street improvements (Washington to Main). Improved sidewalks and installation of Secondary Street streetscape elements. Installation of off-street bike facility.

**6B.** Street improvements (Main to Warren). Improved sidewalks and installation of Secondary Street streetscape elements. Installation of off-street bike facility.

### 7 WARREN STREET

Street improvements (Center to Sycamore). Improved sidewalks and installation of Tertiary Street streetscape elements. Application of bike boulevard street markings and signage.

### 8 SYCAMORE STREET

**8A.** Street improvements (Shawnee to Main). Improved sidewalks and installation of Tertiary Street streetscape elements. Application of bike boulevard street markings and signage. On-street parking will remain unchanged.

**8B.** Street improvements (Main to Warren). Improved sidewalks and installation of Tertiary Street streetscape elements. Application of bike boulevard street markings and signage. On-street parking will remain unchanged.

### 9 BLOCK IMPROVEMENTS (SOUTH OF MAIN ST.)

**9A.** Parking lot consolidation improvements.

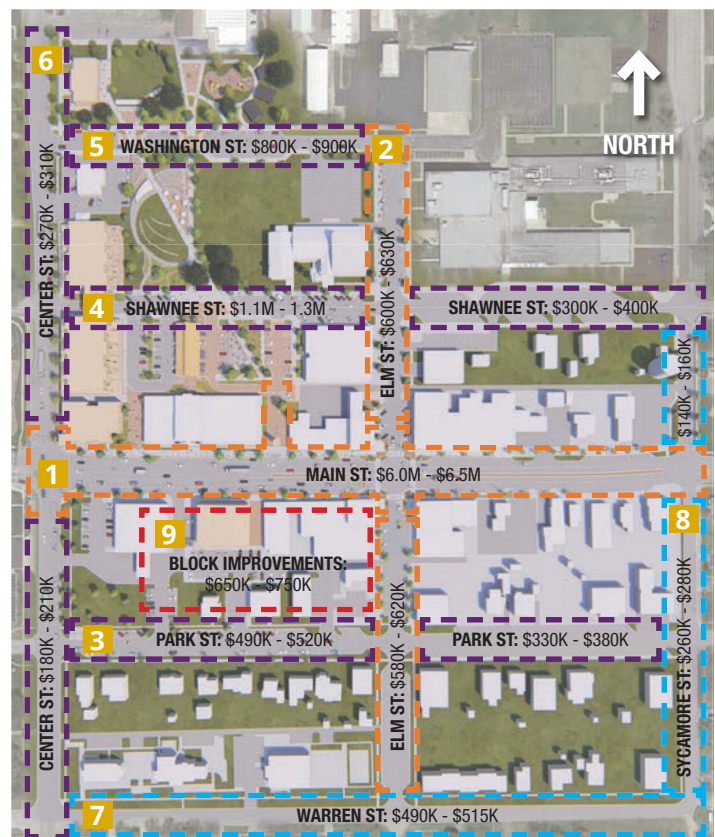


Figure 5.05 - Streetscapes estimated costs

*\*Refer to Page 27 to see complete list of streetscape elements.*



STREETSCAPES				
PROJECT		EST COST*	POTENTIAL FUNDING SOURCES	PARTICIPANTS
PRIMARY STREET				
1	Main Street Improvements <i>(Center to Sycamore)</i>	\$6.0 - \$6.5M	AM, CCLIP	City / KDOT
2A	Elm Street Improvements <i>(Washington to Main)</i>	\$600 - \$630K	TA	City
2B	Elm Street Improvements <i>(Main to Warren)</i>	\$580 - \$620K	TA	City
SECONDARY STREET				
3A	Park Street Mobility Hub	\$110 - \$170K		City / KCATA
3B	Park Street Improvements <i>(Center to Elm)</i>	\$330 - \$360K		City
3C	Park Street Improvements <i>(Elm to Sycamore)</i>	\$330 - \$380K		City
4A	Shawnee Street Improvements <i>(Center to Elm)</i>	\$1.1 - \$1.3M	TA	City
4B	Shawnee Street Improvements <i>(Elm to Sycamore)</i>	\$300 - \$400K	TA	City
5	Washington Street Improvements <i>(Center to Elm)</i>	\$800 - \$900K	TA	City
6A	Center Street Improvements <i>(Main to Washington)</i>	\$270 - \$310K	TA	City
6B	Center Street Improvements <i>(Main to Warren)</i>	\$180 - \$210K		City
TERTIARY STREET				
7	Warren Street Improvements <i>(Center to Sycamore)</i>	\$490 - \$515K	TA	City
8A	Sycamore Street Improvements <i>(Main to Shawnee)</i>	\$140 - \$160K	TA	City
8B	Sycamore Street Improvements <i>(Main to Warren)</i>	\$260 - \$280K	TA	City
9A	Block Improvements <i>(Parking lot improvements)</i>	\$650 - \$750K		Public / Private Partnership

**Figure 5.06** - Streetscapes implementation matrix

\*Estimated costs are based on 2019 values and should be updated routinely to account for inflation and other economic factors.

\*\* For definitions of potential funding sources, see Figure 5.04, on Page 38.

## 5.2 IMPLEMENTATION STRATEGY

The Gardner Destination Downtown vision encompasses much of the Downtown core and has many exciting components that will transform the community. However, because of municipal financial realities, limited funding opportunities, property acquisitions, and the need for public / private partnerships, the overall vision will likely not occur simultaneously. Although the completed improvements are designed and will function as a holistic and interrelated environment, it is unlikely that the entire project could be funded and built all at once. Several factors will drive future decisions about how the plan can be implemented in phases over a period of time with varied funding sources. This plan will serve as the blueprint for construction even if staff or elected officials change over time. Implementation will require strategies for four key factors:

- Consistency of Leadership
- Funding
- Partnerships
- Phasing

### CONSISTENCY OF LEADERSHIP

The key components of the final recommended plan are designed so that parts of the project can be completed no matter what order may be the most beneficial for the City when available funding or partnership opportunities arrive. A designated staff member or department has to be responsible for keeping track of everything and the City must be ready to act decisively when opportunities arise.

The City of Gardner and the appropriate staff or department need to direct the implementation of the plan and/or its elements, including the coordination of various funding sources and the partnership relations with the surrounding property owners, many of whom are other governmental entities or not for profit organizations with different decision-making processes.

### FUNDING

Besides standard funding options through the City's Capital Improvements Plan or established Community Improvement Districts, some creative funding strategies might be possible. For instance, if a local health care facility promotes quality of life and preventive health programs, they may be interested in the public park space or in adding fitness equipment throughout the spaces, etc. Since the Johnson County Fair Board needs to make facility improvements, perhaps the County would consider a one-time investment to help fund the Elm Street improvements along with a new gateway to the Fairgrounds, or any county-oriented space for satellite services that might provide an advantage for their operations. AARP grants or Safe Walk to School grants could be explored for the ADA accessibility improvements, streets, sidewalks, etc.

### PARTNERSHIPS

There are multiple stakeholders and various property owners besides the City that have all participated in this community planning process. There have been numerous individual meetings with surrounding property owners to identify and work through their varying concerns and to develop positive ongoing communications regarding the plan, its

progress and the implementation issues. Partners have included the Johnson County Fair Board, the Johnson County Library, the Gardner School District, First Presbyterian Church, the owners of Blazer's restaurant, KCATA, the Central Bank of the Midwest and potential developers.

Partnership Agreements with these key property owners should be developed by the City regarding public use of or acquisition of property. Options might include:

- Long term leases
- Use of public easements
- Acquisition of property
- Memorandum of Understanding
- Agreements on days/weeks/ months set aside for regular or special uses and activities needed by the partner.
- Maintenance Agreements
- Notification of closed streets, temporary parking measures, signage, etc. for special events

Being alert for special partnership opportunities with other Gardner Institutions, organizations and the private sector that can benefit multiple entities will be important for Gardner to capitalize on to bring the full plan into fruition.

### PHASING

In order to provide city leaders with direction on community priorities for implementation, the Steering and Implementation Committee participated in a scenario building workshop. These four scenarios offered alternative realities that could affect the City's decision on how and when to proceed with certain projects. The results of this workshop can be found at the end of this section.

It is unlikely that the phasing of the project will happen in a linear manner, so the City should be prepared to act on whatever opportunities arise first. The components of the plan break the project into feasible sections that can help with identification of the amenities that are attractive to certain funding options and can be constructed separately, if necessary. The plan can be used as marketing to solicit outside funding. The City can also identify the components that will require public funds and develop a strategy for funding those components through the existing budgeting process, the Capital Improvements Plan or other means.



Figure 5.07 - Implementation scenario building workshop

## IMPLEMENTATION SCENARIO WORKSHOP SUMMARY

### SCENARIO 1

The City receives a grant that, along with available city investment, will fund a portion of the streetscape and intersection improvements within the Downtown plan project area.

- *Make big moves first - focus on Main and Elm intersection so that people can see beneficial change and become engaged.*
- *ADA accessible stall in front of City Hall is important - could be western extents of initial investment area (accessibility is hardest in front of City Hall).*
- *Extend the street / streetscape improvement project out from Main and Elm intersection a 1/2 block in each direction to incorporate ADA improvements.*
- *Iconic vertical element would be great to include in initial investment.*
- *2nd phase idea - continue north on Elm and do all of Washington Street.*

### SCENARIO 2

The Parks and Recreation Master Plan is concluded, and the City determines that making improvements to Cornerstone Park should be the first visible outcome of the plan. Unfortunately, the available funding that has been identified will only cover the cost to either build "Cornerstone Hall" OR the other proposed park improvements.

- *Proposed building would be exciting and a great new feature for Cornerstone Park and Parks Department. We could have numerous events from birthdays, senior events, etc.*
- *What adds the most value to the park - events (once or twice a week) vs everyday community use.*
- *Adding a new building provides event space, however, if we don't have funds for the park where would people congregate outside during events? Makes sense to update the park amenities first and add the building second.*
- *The community impact would be amazing with a truly inclusive playground and fountain area for families and kids.*
- *Playground needs to be a destination feature to draw the community Downtown and have a great synergy with the Aquatics Center.*
- *The Elementary School has a special needs program and would use and appreciate an inclusive destination playground.*
- *Everyday community impact of site improvements (playground, open space and fountain) far outweigh the weekend events in the proposed building.*
- *All Cornerstone improvements (park and building) must be done right with appropriate funds to accomplish that goal.*

### SCENARIO 3

The City concludes the space study for City Hall. The study prioritizes improvements, including changes to exterior accessibility and parking that are consistent with the vision for new public spaces adjacent to City Hall. Additionally, the Gardner Library has embraced the idea of improvements to their parking lot as consistent with the plan and has agreed to partner in implementation. However, a plan for relocating the group home to a new facility has not yet been defined.

- *Work harder to relocate group home... don't wait.*
- *ADA parking at City Hall is great.*
- *Area that is public control, need to act on this as soon as possible (1st domino).*
- *Focus on ground level improvements between group home and library - market can be in the future.*
- *Seek external sponsors and partners whose missions fit the goals of this plan.*

### SCENARIO 4

A developer has approached the City with a vision to implement a new mixed-use development starting at the northeast corner of Main Street and Center Street and continuing north to Blazer's property. They have a plan to relocate the group home to a new facility as part of this development. Moving forward with their new development is contingent upon the City making significant investments in the proposed public space and amenities.

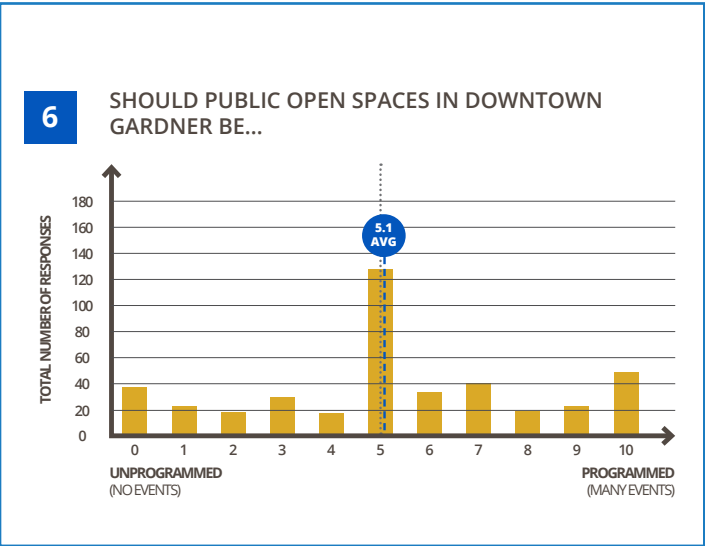
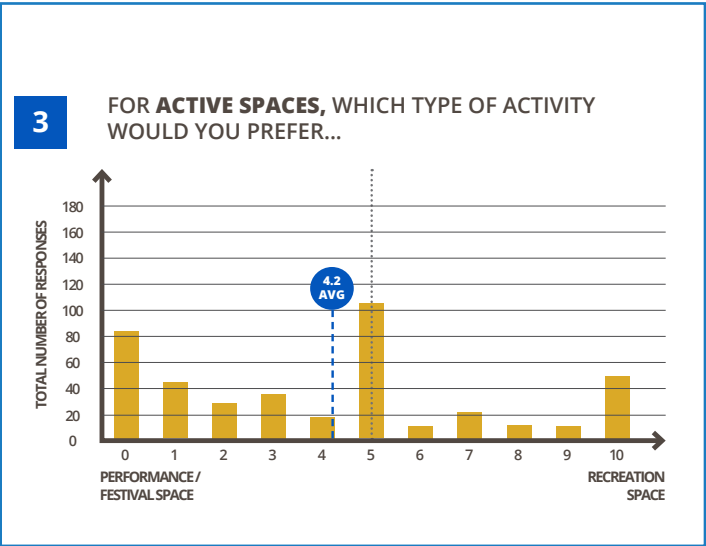
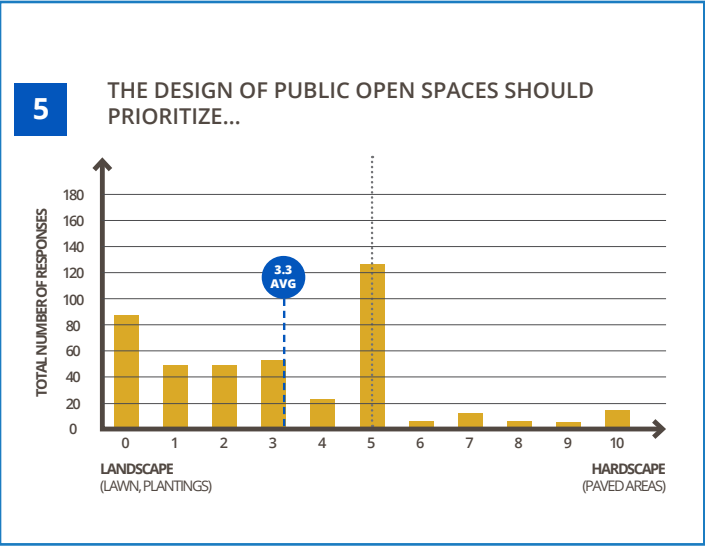
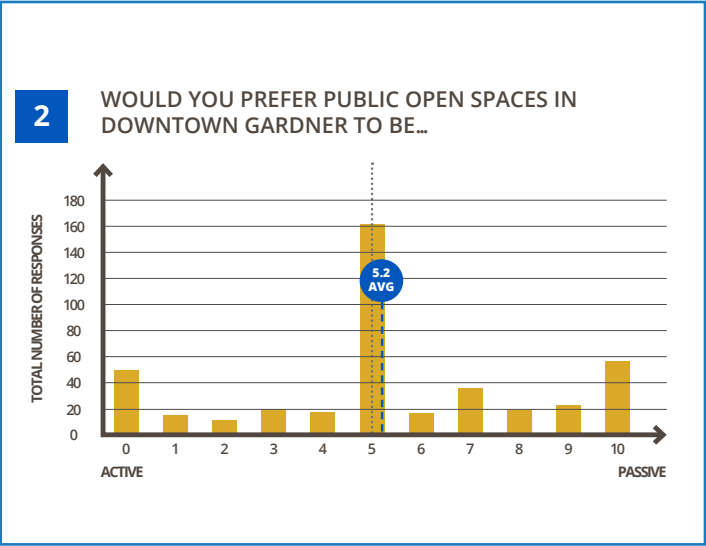
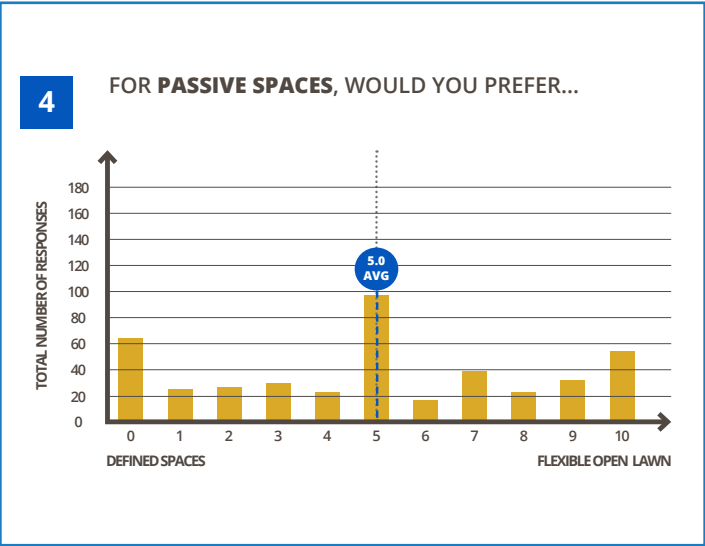
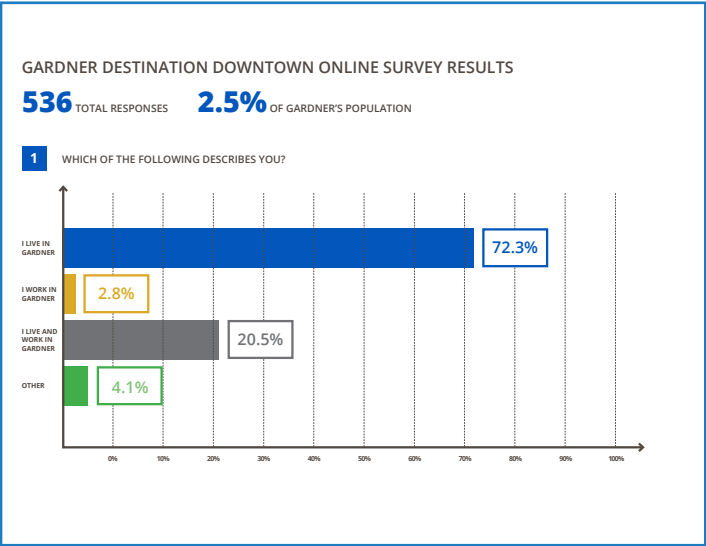
- *This would be a good catalyst to get support for the public improvements around it.*
- *Work with developer to relocate the group home to a better location / facility.*
- *Like the mixed-use idea - would it also be possible for retail shops to have access onto the park as well as the streets?*
- *Explore CID, TIF, other special district possibilities to help pay for improvements.*
- *Is it possible to use a Downtown Impact District tool to capture revenue for a period of time where any improvement Downtown (not just here) would pay a percent into a fund that helps pay for the park improvements?*
- *Build the market structure first because the City owns most of the property already.*
- *Get something done in Cornerstone Park area right away (at least part of it).*
- *Maybe we should consider charging for parking for certain events that generates a large attendance (help to raise revenue).*





## APPENDIX

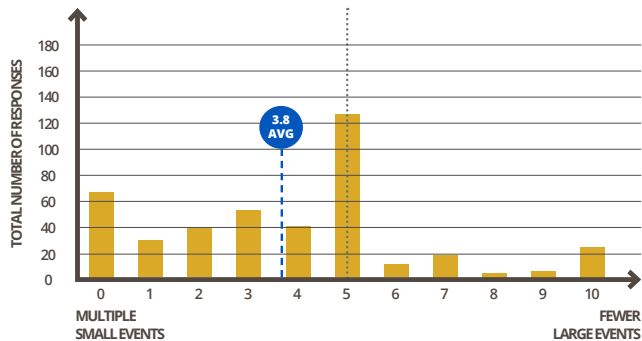
ONLINE SURVEY RESULTS





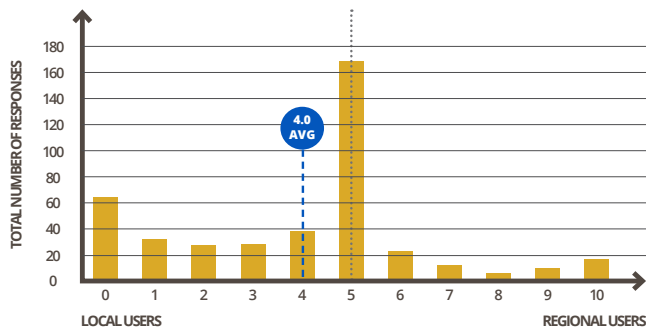
7

WHAT SIZE EVENT OR GATHERING SHOULD BE PRIORITIZED IN DOWNTOWN GARDNER...



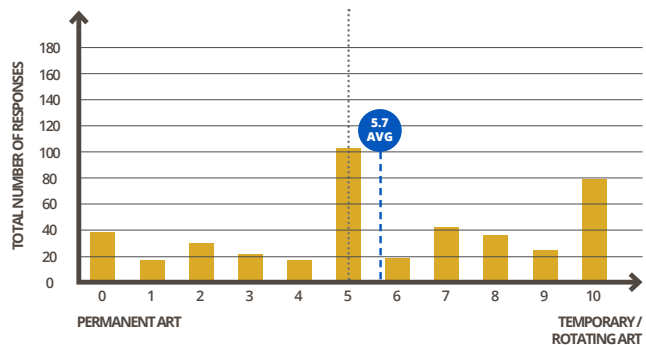
8

SHOULD PUBLIC OPEN SPACES IN DOWNTOWN GARDNER ATTRACT...



9

IF ART IS INCORPORATED INTO DOWNTOWN GARDNER'S PUBLIC OPEN SPACES, WHAT TYPE OF ART SHOULD IT BE...



FARMERS MARKET

79.9%

325



WALKING PATHS

63.8%

260



INTERACTIVE WATER FEATURE

60.9%

248



FOOD TRUCKS

55.3%

225



AMPHITHEATER

53.6%

218



NATIVE FLOWER GARDEN

43.7%

178



PUBLIC ART

43.5%

177



MINI GOLF

42.3%

172



DOG PARK

39.6%

161



BOTANICAL GARDEN

35.6%

145



NATURE PLAY

34.9%

142



TRADITIONAL WATER FEATURE

32.9%

134



PLAYGROUND

32.7%

133



BAND STAND

32.2%

131



OPEN LAWN

30.1%

126



FITNESS AREA / STATIONS

24.8%

101



SHELTER / PAVILION

24.6%

100



ICE RINK

20.4%

83



BOCCIE COURT

17.7%

72



FIRE PIT

17.4%

71



COMMUNITY GARDEN

17.0%

69



READING ROOM

16.7%

68



OVERSIZED GAMES

16.0%

65



OUTDOOR CLASSROOM

15.7%

64



HISTORIC MARKER

15.5%

63



BASKETBALL COURT

15.5%

63



VOLLEYBALL COURT

15.2%

62



DISCOVERY GARDEN

14.7%

60



SKATE PARK

14.7%

60



VETERANS MEMORIAL

14.3%

58



ROSE GARDEN

13.3%

54



HAMMOCKING

12.8%

52



PICKLEBALL COURT

10.3%

42



HORSESHOES

9.6%

39



TENNIS COURT

6.9%

28



PING PONG

5.2%

21

# INITIAL CONCEPTS

## A INCREMENTAL CHANGE

In this concept, the least amount of land was available for development between Washington Street and Shawnee Street. A linear garden and walkway along the west side of the existing open space serves as a connector between Cornerstone Park and the area behind City Hall. Cornerstone Park sees an extensive redesign while keeping the successful features such as the playground, shelters and mature trees. The area behind City Hall transforms into a flexible open green space that can serve as a venue for festival or musical performances. Also nearby is a permanent farmers market facility and plaza area.

## B SPINE

The goal of the spine concept was to facilitate a visual and physical connection between Main Street and Cornerstone Park through a series of civic gathering spaces. The parking and entrance road immediately to the east of City Hall is removed and a pedestrian plaza featuring an iconic vertical element would bring the overall design to the forefront of the Main Street Corridor. Heading north, a Public Market is directly behind City Hall, and when not being used, the parking located here would fulfill the needs of city employees and residents alike. To the north of Shawnee Street, the spine leads into a larger open green space and amphitheater venue. Buffered by a botanical garden and plaza seating areas, this space is meant for festivals and concerts and varying sizes. Similar to the first concept, the Spine also proposed improvements to Cornerstone Park that seek to enhance this already established asset in the community. New residential townhome units along Center Street and mixed-use buildings at the corner of Center and Main and along the south side of Main Street also look to bring new residential and commercial development to Downtown Gardner.

## C PROMENADE

The Promenade achieves many of the same goals as the Spine but in a slightly different way. Leading directly out the north door of City Hall, a path extends to Cornerstone Park and ties all three spaces together. Along that path are a farmer's market, amphitheater and event lawn, nature playground, and finally a revitalized Cornerstone Park. Residential and mixed-use developments, very similar to the Spine, remain along Center and Main Street.



Figure I - Initial concept 'Incremental Change'



Figure II - Initial concept 'Spine'



Figure III - Initial concept 'Promenade'



## PUBLIC SPACES - ENGAGEMENT RESULTS

These three initial concepts were shared at a public meeting and attendees were asked to review and form an opinion on what they were observing. They were given a series of dots of varying colors and those dots represented the following:

- 1st Choice
- 2nd Choice
- 3rd Choice
- Elements You Like

The vote totals to the left reflect a combination of both the public feedback and Steering Committee input. Overall the public favored Concept 1, Incremental Change, while the Steering Committee agreed upon Concept 3, the Promenade.

The element that received the most positive feedback was the idea of a permanent farmers market structure. Currently, the Gardner Farmers Market meets on a gravel lot owned by the Johnson County Fair which is not very prominent within the context of the Downtown area. Having a permanent structure would help establish a more vibrant merchant and customer base. The public and steering committee agreed that the area immediately behind City Hall would be an ideal location for the market to establish itself.

Additionally, the idea of proposing residential development along Center Street received positive feedback as it would add population to the center of the City and provide a buffer between the public green space and its associated activity with the nearby residential neighborhoods on the west side of Center Street.

Other elements that were well received included the multi-use community building, public amphitheater space, mixed-use development and play features.

Often during a public process such as this, not all sides agree on one concept as was the case for this project. It was the mission of the planning team to hear and process all the feedback given and produce a vision that satisfies as many opinions as possible.

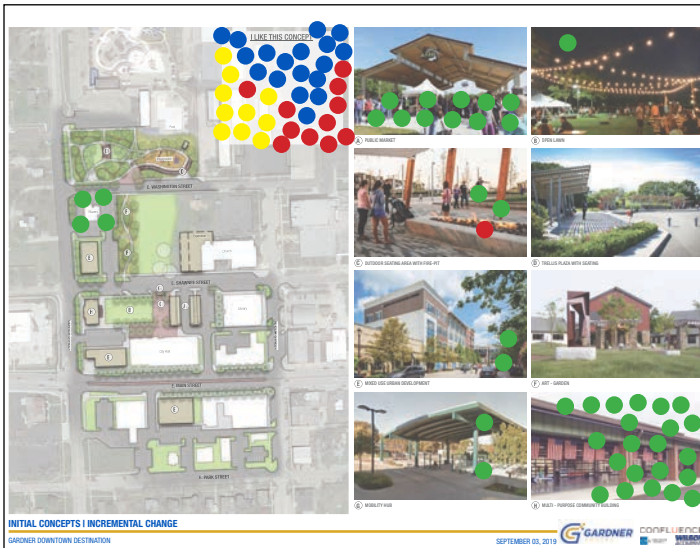


Figure IV - Initial concept 'Incremental Change' vote totals



Figure V - Initial concept 'Spine' vote totals

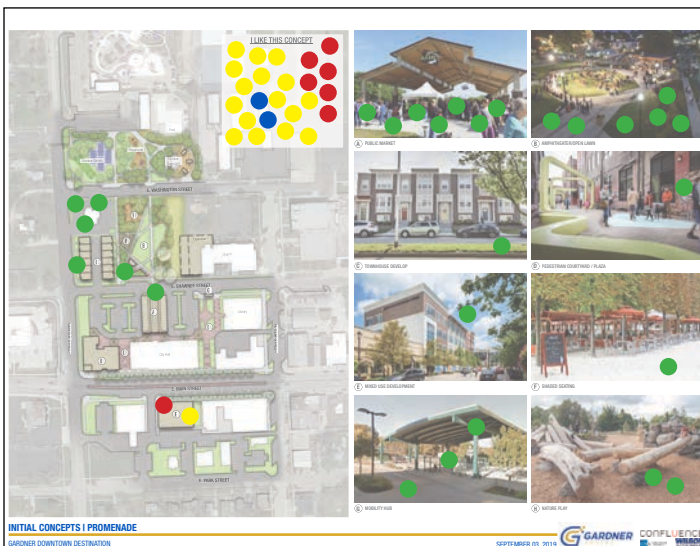


Figure VI- Initial concept 'Promenade' vote totals



## STREETSCAPE - INITIAL CONCEPTS

During the 2018 Gardner Main Street Corridor study, it was established that community members preferred a more traditional aesthetic when it came to streetscape elements. It was agreed upon that a traditional study would fit nicely with the character of Downtown.

For the purposes of this study, we further developed design concepts for the streetscapes and a 'kit of parts' for streetscape elements based off the original feedback given in 2018.

The first step in developing those concepts further, we asked Steering Committee members to pick a theme they thought best represented Gardner. The themes were 'Prairie Trails' and 'Wings and Fins', both of which represent a physical, natural or historical component that makes Gardner unique.

Overwhelmingly, the Steering Committee selected 'Prairie Trails' as the preferred theme. The general consensus was that Gardner's location on the fringe of the Flint Hills and along the historic westward expansion trails was a story that needed to be told.



Figure VIII - Traditional streetscape character image

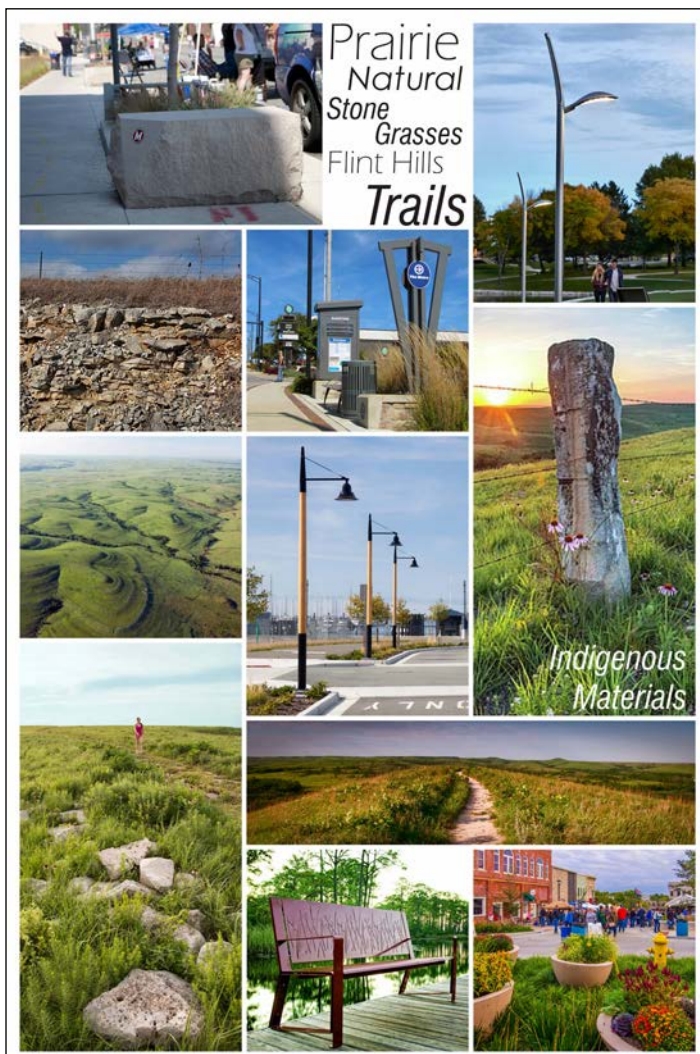


Figure VII - 'Prairie Trails' character images



Figure IX - 'Wings and Fins' character images



## TOWN SQUARE

The Town Square concept uses many of the same materials that are found in the Downtown Gardner such as brick and limestone to create a comfortable and familiar feel for pedestrians and vehicles alike. Curb bump outs allow for landscape beds, gateway markers and seating opportunities. Within the proposed median, small limestone blocks provide additional visual interest and speak to the natural elements of Kansas. This concept was selected as the preferred approach.

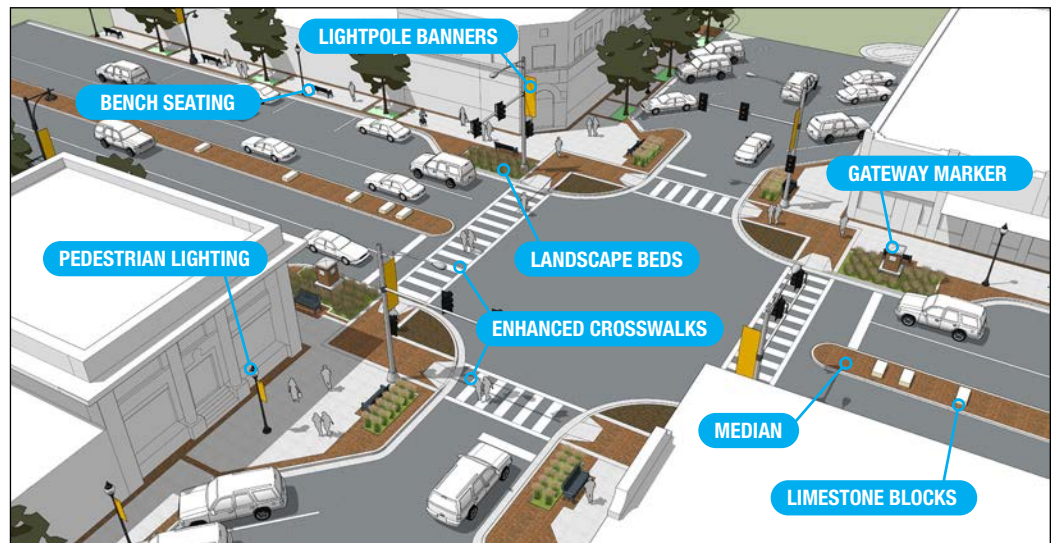


Figure X - 'Town Square' streetscape design concept

## SOFTENED CORNERS

A more contemporary approach, the Softened Corners concept, speaks to Gardner's more recent history of aerodynamics and modern transportation networks. A softer color palette and rounded hardscape features give this concept a much more fluid character. This language is also embodied in the seating elements, landscape beds and pedestrian lighting.

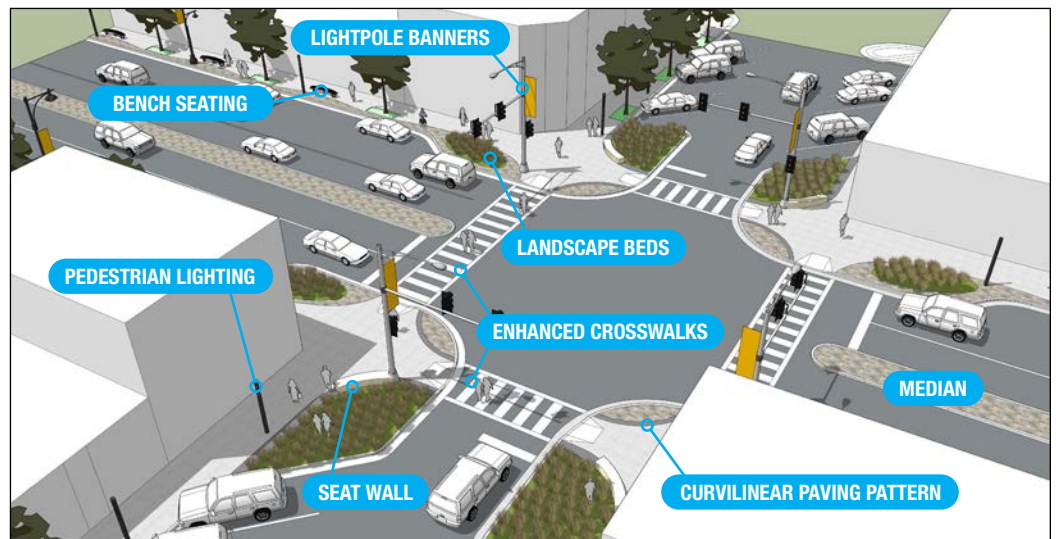


Figure XI - 'Softened Corners' streetscape design concept

## RADIATING SPACES

The Radiating Spaces concept seeks to unify the entire Main and Elm Intersection with a dynamic system of paving patterns and landscape features. This concept plays off the idea that this intersection is the historic and contemporary center of Gardner and the rest of the City radiates out from this location. Seat walls of varying sizes, as well as benches, provide ample places for gathering outside Downtown businesses.

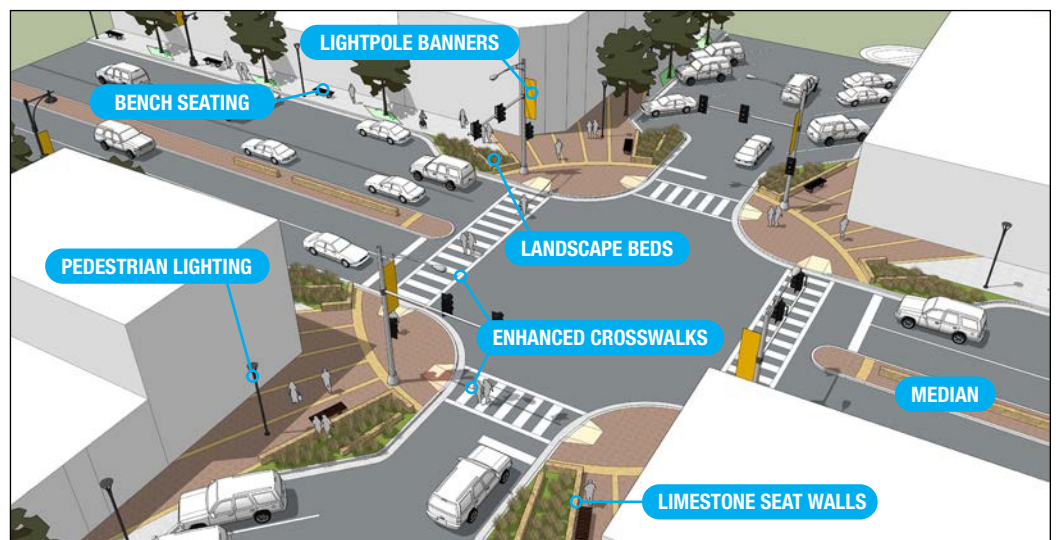


Figure XII - 'Radiating Spaces' streetscape design concept





Figure XIII - Plan of proposed public space and streetscape improvements in Downtown Gardner



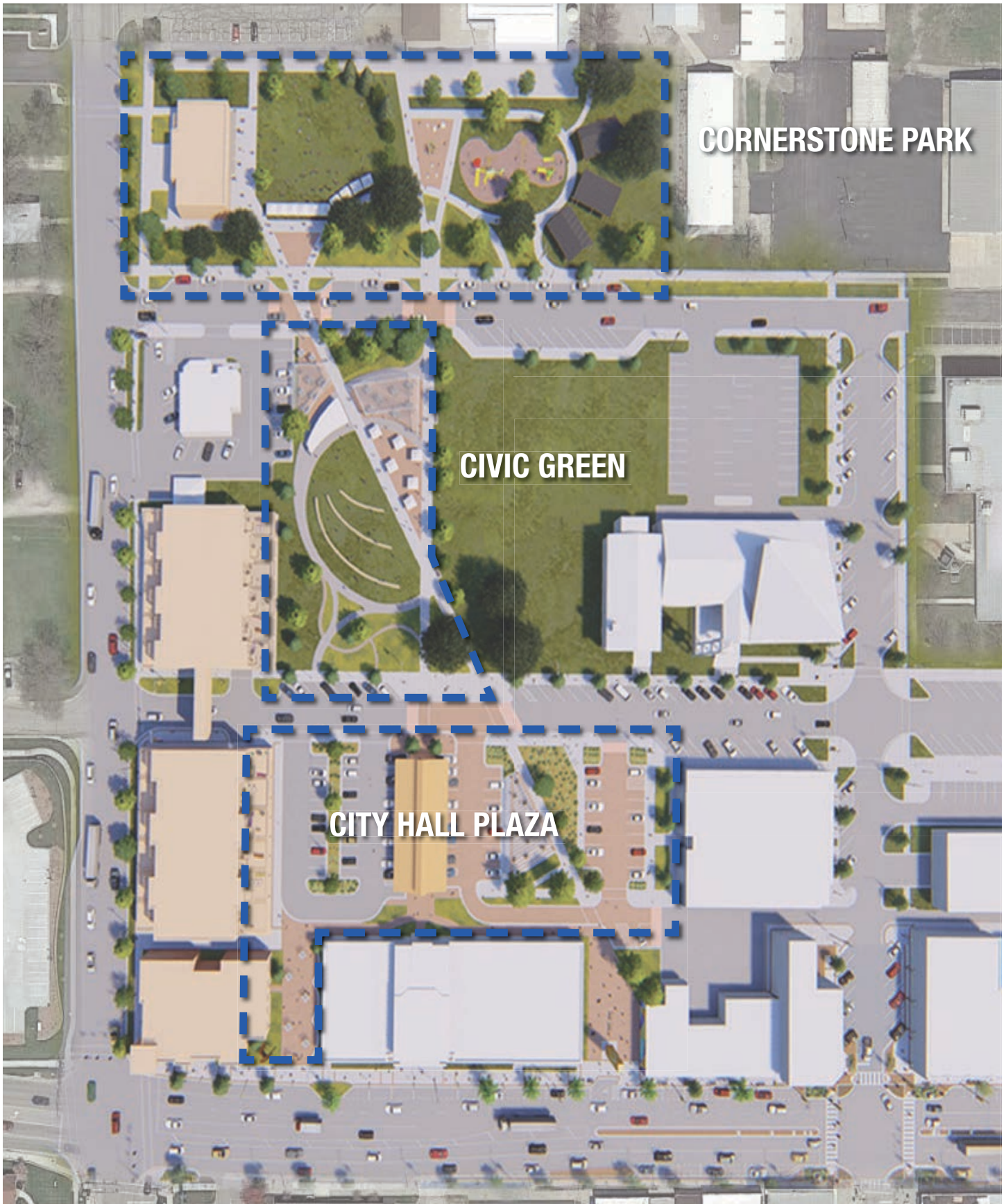


Figure XIV - Plan view of proposed public spaces in Downtown Gardner





Figure XV - Parking Inventory. Streets and lots not called out on this plan, indicate no changes in quantity of parking stalls.

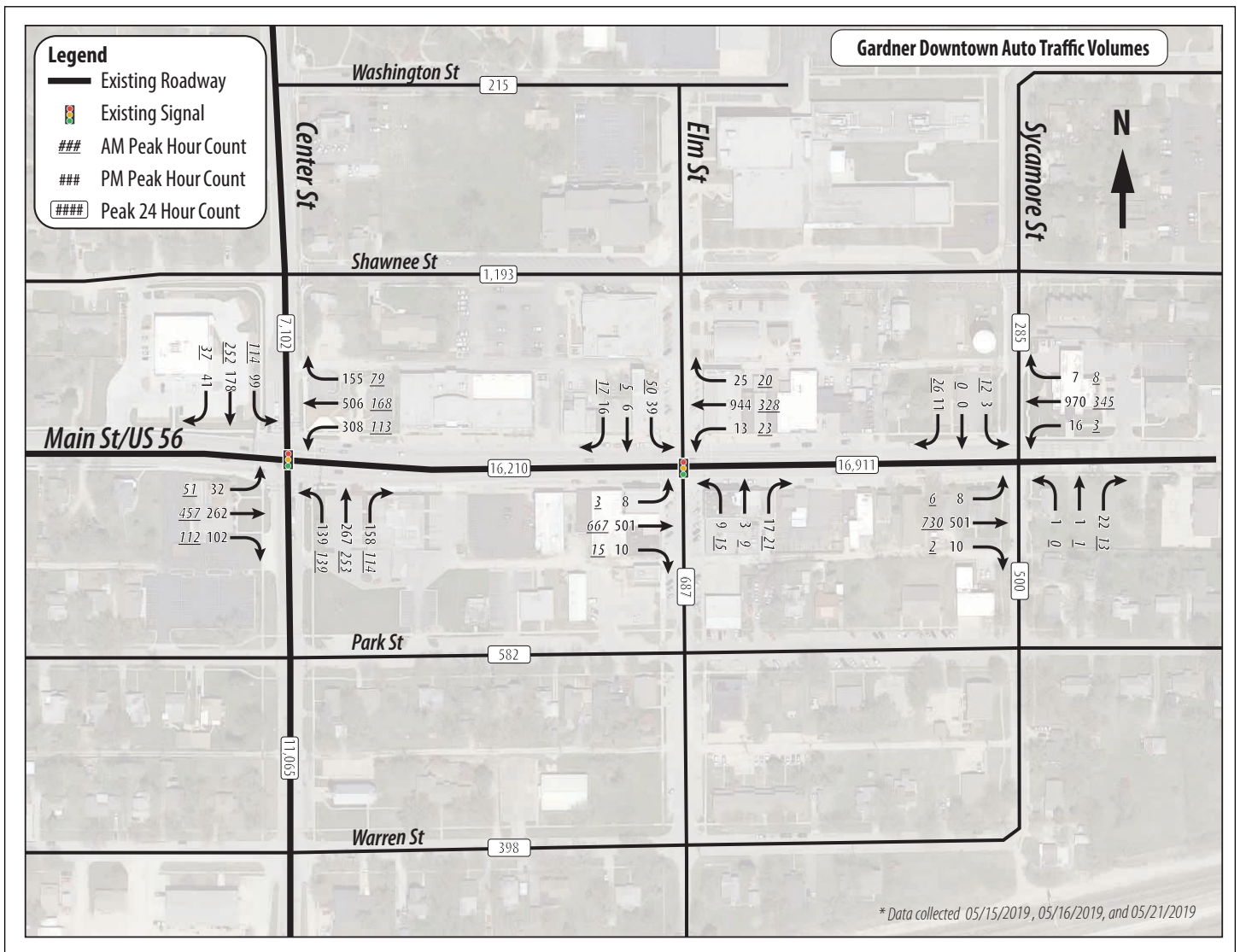


Figure XVI - Traffic analysis for Downtown streets



# CITY OF GARDNER COMPREHENSIVE PLAN

PREPARED BY: HOUSEAL LAVIGNE ASSOCIATES || ADOPTED SEPTEMBER 15, 2014

Adopted by Planning Commission Resolution PC-14-02 on July 29, 2014 and  
City Council Ordinance No. 2457 on September 15, 2014

Revised:

Planning Commission Resolution PC-16-02 on May 24, 2016 and  
City Council Ordinance No. 2517 on June 20, 2016

Planning Commission Resolution PC-17-01 on July 25, 2017 and  
City Council Ordinance No. 2553 on September 18, 2017

Planning Commission Resolution PC-17-02 on August 27, 2017 and  
City Council Ordinance No. 2554 on September 18, 2017

Planning Commission Resolution PC-18-01 on March 27, 2018 and  
City Council Ordinance No. 2574 on April 16, 2018

Planning Commission Resolution PC-18-02 on May 22, 2018 and  
City Council Ordinance No. 2582 on June 18, 2018

Planning Commission Resolution PC-20-01 on February 25, 2020 and  
City Council Ordinance No. \_\_\_\_ on March 16, 2020



# **AMENDMENTS TO THE COMPREHENSIVE PLAN FOR THE CITY OF GARDNER, KANSAS, AMENDING THE COVER PAGE, CHAPTER 1 INTRODUCTION, CHAPTER 3 PUBLIC PARTICIPATION, CHAPTER 4 VISION, GOALS & OBJECTIVES, CHAPTER 5 LAND USE & DEVELOPMENT, CHAPTER 9 IMAGE & IDENTITY, CHAPTER 10 SUSTAINABILITY FRAMEWORK, AND CHAPTER 11 IMPLEMENTATION AND ADOPTING BY REFERENCE THE GARDNER DESTINATION DOWNTOWN: A PLACEMAKING AND MOBILITY ENHANCEMENT PROJECT.**

Amend the cover page to include all adopted amendments to date.

## **CHAPTER 1. INTRODUCTION**

### **ORGANIZATION OF THE COMPREHENSIVE PLAN**

The Gardner Comprehensive Plan is divided into the following chapters:

1. **Introduction** – a description of the purpose of the plan, process, and content.
2. **Community Profile** – a snapshot of the City of Gardner, as it is today, that establishes the basis for recommendations in subsequent chapters.
3. **Public Participation** – a summary of the public outreach activities that took place during the comprehensive planning process, and the outcomes they produced.
4. **Vision, Goals, and Objectives** – an articulation of the shared vision for Gardner and a description of the goals and objectives for achieving the community's vision.
5. **Land Use Plan** – a future land use map for the City of Gardner and subsequent recommendations for the residential, commercial, and industrial areas of the community.
6. **Community Facilities and Services** – recommendations related to the maintenance of existing facilities and the future expansion of community facilities and services.
7. **Transportation and Mobility** – recommendations related to roadways, bicycle and pedestrian mobility, transit, and airports.
8. **Environmental Features and Open Space** – recommendations for enhancing the benefits of parks, open space, and environmental features which help to define the community's quality of life.
9. **Image and Identity** – recommendations related to the character, image, and appearance of the community including streetscape, gateways, development character, and more.
10. **Sustainability Framework** – a foundation of sustainability policies, initiatives, and program development.
11. **Implementation** – specific actions required to carry out the new Comprehensive Plan including recommendations regarding regulations, economic development strategies, priority improvement projects, general administration, and updates to the Plan.
12. **Capital Improvements** – a compilation of the various infrastructure and facility/service needs related to growth and development, with schedules of capital improvements for 5 years, 10 years, and 10+ years. (Ord. 2553)

**Appendix A: Action Plan** – a table referencing all goals, objectives, and recommended actions, including status updates. (Ord. 2554)

The following plans have been adopted and incorporated by reference:

**I-35 & Gardner Road Interchange Subarea Plan** – adopted by Resolution No. PC-16-02 by the Planning Commission on May 24, 2016, and adopted by Ordinance No. 2517 by City Council on June 20, 2016.

**Gardner Main Street Corridor Plan**, with associated **Gardner Main Street Corridor Market Analysis** – adopted by Resolution No. PC-18-01 by the Planning Commission on March 27, 2018, and adopted by Ordinance No. 2574 by City Council on April 16, 2018.

***I-35 & 175th Street Interchange Subarea Plan*** – adopted by Resolution No. PC-18-02 by the Planning Commission on May 22, 2018, and adopted by Ordinance No. 2582 by City Council on June 18, 2018.

***Gardner Destination Downtown: A Placemaking and Mobility Enhancement Project*** – adopted by Resolution No. PC-20-01 by the Planning Commission on February 25, 2020, and adopted by Ordinance No. \_\_\_\_ by City Council on March 16, 2020.

## CHAPTER 3. PUBLIC PARTICIPATION

Public participation and input is the cornerstone of the Comprehensive Plan. The Plan is founded upon the thoughts, concerns, and aspirations of the residents and businesses that call Gardner home. In this way, the City of Gardner Comprehensive Plan serves as a true reflection of the City.

This chapter of the Comprehensive Plan provides a summary of the range of public outreach events that occurred throughout the planning process. It also includes an overview of online outreach efforts that included resident and business surveys as well as a web-based mapping tool. The culmination of these results provided the foundation upon which the Gardner Comprehensive Plan was built.

Ongoing public participation and input has been obtained as part of subsequent planning efforts such as the *Gardner Main Street Corridor Plan*, *Gardner Destination Downtown: A Placemaking and Mobility Enhancement Project plan*, and the subarea plans that are adopted by reference as part of the Comprehensive Plan. Detailed information on methods of public engagement and input received is contained within those plans. The ideas obtained through these separate planning efforts deepen understanding of issues identified by the Comprehensive Plan relating to community facilities, land use, transportation and mobility, community character, and economic development. (Ord. 2574, Ord. \_\_\_\_)

## CHAPTER 4. VISION, GOALS & OBJECTIVES

### GOALS & OBJECTIVES

The City of Gardner Comprehensive Plan provides a road map for policies that will guide the City over the next 15-20 years. The Goals and Objectives help organize the Plan and provide the conceptual framework for more detailed recommendations.

Goals are overarching principles that outline an intended outcome; they are broad and long-range. Objectives are specific actions and policies that should be undertaken and/or adopted by the City of Gardner to help achieve its goals.

The Goals and Objectives are categorized as follows:

- Neighborhoods and Housing
- Commercial Areas
- Industrial Areas
- Growth and Annexation
- Transportation and Mobility
- Community Facilities
- Open Space, Recreation, and Environmental Features
- Community Character
- Sustainability

Since adoption in 2014, steps have been taken to address the goals and objectives of the Comprehensive Plan. For example, the adoption of the *Gardner Land Development Code* in 2016 (Ord. 2518) satisfies a wide range of objectives (here indicated by the designation “(LDC)” after the recommendation). As part of the 2017 amendments (Ord. 2554), the City added Appendix A: Action Plan, which includes a summary table of all Comprehensive Plan recommendations. In 2018, an executive summary of achieved tasks (to be updated annually) was added to Appendix A, and these achievements are reflected in the status column of the Action Table.



Furthermore, subsequently adopted area plans (adopted by reference as part of this Plan) have presented additional recommendations that are related to the goals and objectives of the Comprehensive Plan. These plans provide more specific, detailed recommendations and guidance for land use decisions in those planning areas. Recommendations that are explored in greater depth in the *Gardner Main Street Corridor Plan* are here indicated by the designation “(MCP)” after the recommendation. (Ord. 2574) Recommendations that are explored in greater depth in the *Gardner Destination Downtown: A Placemaking and Mobility Enhancement Project plan* are here indicated by the designation “(GDD)” after the recommendation. (Ord. \_\_\_\_)

## COMMERCIAL AREAS

**Goal:** Promote the City’s commercial character by supporting local businesses and creating a thriving Downtown area and commercial nodes integrating mixed-use and pedestrian-oriented design and development. (MCP) (GDD)

### Objectives:

- Allow for greater flexibility within the zoning and development controls to promote business growth and retention (LDC, MCP)
- Promote Downtown Gardner businesses through marketing and branding strategies and local partnerships including the Chamber of Commerce and similar organizations
- Create a mixed-use, pedestrian-oriented commercial district in Downtown and Main Street through streetscape, infrastructure upgrades, and economic development efforts (MCP) (GDD)
- Work with the Kansas Department of Transportation to redesign Main Street (MCP) (GDD)
- Attract hotels and hospitality-related commercial uses to support corporate and office developments
- Promote commercial growth along Main Street and at the I-35 interchanges to provide neighborhood and regional commercial areas with a diverse range of retail and commercial uses and services (MCP)
- Create a specific area plan for Downtown that considers future land use and development opportunities, historic preservation, transportation, and streetscape (MCP) (GDD)

## TRANSPORTATION & MOBILITY

**Goal:** Design a transportation network that provides safe and efficient access for all modes of travel between residential areas, businesses, and civic and recreational facilities while ensuring streets, sidewalks, and trails are well-maintained.

### Objectives:

- Work with the Kansas Department of Transportation to assess the viability of redesigning and/or rerouting US-56 to a location outside of Downtown (MCP)
- Implement a Safe Routes to School Program that allows children to safely travel to local schools by foot or bicycle
- Adopt a Complete Streets policy so future growth and development is safe and accessible for all users (LDC)
- Make critical pedestrian connections to community facility destinations, including the integration of trails as outlined in the Parks System Master Plan (MCP) (GDD)
- Require full sidewalk build-out within a reasonable amount of time (e.g. 2 years) after the beginning of new development
- Develop a strategy to improve existing pedestrian facilities to be compliant with ADA guidelines (GDD)
- Work with the railroad to improve rail crossing delays throughout the City using improved signalization
- Work with Johnson County to improve access to local and regional public transportation
- Coordinate responsiveness with the Gardner Police Department and local fire protection agencies as new growth and development occurs
- Review potential growth areas for new, rerouted, or reclassified roads

## COMMUNITY FACILITIES

**Goal:** Ensure high quality and dependable public services and facilities including the Public Works and Parks and Recreation Departments, police and fire protection, Gardner-Edgerton Unified School District, Johnson County Library, and other community-based facilities.

### Objectives:

- Develop a new Gardner Police and court services facility to support staff and operations, and service the growing and evolving population throughout the community
- As growth occurs, ensure that police staffing adequately serves the needs of the community
- Work in partnership with local fire protection agencies to ensure that existing and growth areas are adequately serviced including the availability of water for fire suppression
- Establish a community center to provide a central location for meetings and events particularly for youth and senior programs (MCP) (GDD)
- Foster increased communication and trust building with residents and businesses through a multi-media campaign that includes outreach to faith-based organizations
- Support the Gardner-Edgerton Unified School District in their ongoing efforts to monitor enrollment rates to plan for potential growth and facility expansion
- Consider alternative locations for new infrastructure that would allow for the planting of trees within the parkway to enhance the public streetscape (MCP)

#### OPEN SPACE, RECREATION & ENVIRONMENTAL FEATURES

**Goal:** Create a balanced park system that provides neighborhood, community, and regional parks that are connected through an extensive trail and greenway network.

##### Objectives:

- Ensure that all residential areas are adequately served by neighborhood and community parks through easements or subdivision regulations (LDC)
- Assess the viability of a new community center based on the recommendation of the 2009 Park System Master Plan, taking into consideration locational needs, access, cost, and local programming and facility needs (MCP)
- Use cluster development and conservation design techniques to conserve natural and open space areas for passive recreation and stormwater management (GDD)
- Update and continue to implement the 2009 Park System Master Plan, acquiring additional parkland and installing new recreational facilities as needed and feasible (GDD)
- Increase public bicycle and pedestrian access to community facilities through connections to the local trail system (MCP) (GDD)
- Maintain the joint use agreements with the Gardner-Edgerton School District to provide additional recreational facilities for residents

#### COMMUNITY CHARACTER

**Goal:** Maintain and enhance the traditional character of Gardner while strengthening the regional image of the community.

##### Objectives:

- Promote Gardner through a multifaceted branding and marketing strategy
- Partner with the Gardner Historical Society to promote the community's heritage
- Install gateway and wayfinding signage at key locations to help define the City's brand and identity (MCP) (GDD)
- Implement a streetscape improvement program to provide a sense of place and identity within key commercial areas, specifically along Main Street, the I-35 interchanges, and the west end of Downtown (MCP) (GDD)
- Promote and require landscaping within commercial areas and require the maintenance and screening of parking lots, service areas, and incompatible uses (LDC)
- Enhance the Johnson County Fairgrounds, including exploring alternative sites, to ensure that the fair continues to be an important part of the City's identity and is compatible with surrounding development (MCP) (GDD)
- Continue to host and support the Johnson County Fair as well as additional events and festivals throughout the year (MCP)

#### SUSTAINABILITY (page 45)

**Goal:** Integrate sustainability measures throughout the City to protect and enhance the natural environment, lower energy demand, and increase healthy living.

#### Objectives:

- Review building codes and amend as needed to incorporate green building design and construction techniques to better manage energy use, stormwater runoff, and other impacts of development
- Encourage energy conservation by residents and businesses to provide individual cost savings and reduce demand on the City's electrical grid
- Require the use of stormwater best management practices (BMPs) for all new development projects while encouraging similar strategies for existing developments *(GDD)*
- Preserve and enhance the City's natural assets such as greenways, wooded areas, Gardner Lake, and the KCPL Prairie Wetland
- Ensure oil extraction processes follow applicable federal, state, and local regulations to protect the health of the natural environment
- Implement policies and regulations that encourage compact development with connected open spaces to support more efficient infrastructure provision *(LDC)*
- Understand trees as critical infrastructure that helps manage stormwater runoff, mitigate heat island effect, improve air and water quality, conserve energy, and provide shade for a safer and more comfortable pedestrian environment, and coordinate utility easement locations so as not to interfere with street tree placement
- Encourage the joint use of utility corridors for utilities, recreation, stormwater management, and appropriate nonmotorized connections
- Support ongoing National Incident Management System (NIMS) training and other hazard mitigation training for all City employees, and provide education for citizens
- Ensure that there are sufficient storm shelters provided for residents of the City's mobile home parks and areas without basements

*(Ord. 2554, Ord. 2574, Ord. \_\_\_\_)*

## CHAPTER 5. LAND USE & DEVELOPMENT

### COMMERCIAL AREAS PLAN

Gardner's commercial areas provide vital goods and services to residents and visitors, include public areas for community events and gathering, and generate significant tax revenue that funds municipal services and infrastructure. They also play a significant role in the image and identity of the City.

Commercial areas in Gardner are generally located along US-56/Main Street, Moonlight Road, and around the I-35/US-56 interchange. This section of the Comprehensive Plan recognizes the varying physical and market characteristics of the different commercial areas and categorizes them into four types:

- Downtown Mixed-Use
- Main Street Corridor
- Community Commercial
- Regional Commercial

It provides policy recommendations to guide reinvestment in existing commercial areas, and ensures future development is reflective of broader community goals. Further policy guidance is found in the *Gardner Main Street Corridor Plan*, and the *Gardner Destination Downtown: A Placemaking and Mobility Enhancement Project plan*, which is adopted by reference as part of the Comprehensive Plan. Recommendations below which are detailed more fully in the *Gardner Main Street Corridor Plan* are followed by the designation "(MCP)". Recommendations below which are addressed by the adoption of the *Gardner Land Development Code (Ord. 2518)* are designated by "(LDC)". *Recommendations that are explored in greater depth in the Gardner Destination Downtown plan are here indicated by the designation "(GDD)" after the recommendation.* *(Ord. 2574, Ord. \_\_\_\_)*

### DOWNTOWN MIXED-USE



Downtown serves as the historic core and center of civic activity for Gardner. The area is anchored by City Hall and includes the Johnson County Library, Senior Center, and historic Bray House. While Main Street serves as the primary east-west corridor, the boundaries of Downtown extend north to Washington Street, south to Warren Street, and east and west to Sycamore Street and Center Street, respectively. A vibrant downtown environment should build upon a mix of retail restaurant, office, and residential land uses, pedestrian-friendly streets, and attractive buildings and architecture.

#### MAIN STREET CHARACTER

As the primary thoroughfare through Downtown, Main Street plays a significant role in conveying the image of the City center, and the community as a whole. This area should continue to accommodate mixed-use or commercial development that is built to the front property line and pedestrian friendly in terms of façade design, massing, and streetscape. Between Sycamore and Center Street, the early 1900's traditional architecture establishes a streetwall that helps to define the space and provide the framework for a pedestrian-oriented environment. However, this streetwall is soon broken by surface parking lots and inconsistent front setbacks. Future development and design of Downtown should consider the following actions and policies:

- Work with property owners to procure funding for the rehabilitation of structures and facades in Downtown (*MCP*)
- Maximize the use of underutilized portions of the public realm for landscaping, wayfinding signage, historic markers, benches, and other streetscape amenities (*MCP*) (*GDD*)
- Explore opportunities to reconfigure Main Street to accommodate more substantial streetscape elements including medians, reconfigured parking, and new traffic patterns for a more pedestrian- and business-friendly Downtown (*MCP*) (*GDD*)
- Continue to use alleys to host utility operations, allowing Main Street to remain clear of utility poles and lines
- Support the creation of a business improvement district to help fund public realm maintenance, marketing, events, security, and beautification projects (*GDD*)
- Modify development regulations to ensure that commercial properties provide adequate screening against nearby residential properties (*LDC*)
- Conduct a historic survey of Downtown and the surrounding residential neighborhoods to reestablish a Downtown Enhancement District

#### DOWNTOWN EDGE

The periphery of the Downtown includes a variety of uses, including parking lots that support uses on Main Street, secondary commercial activities, government or civic uses, and varying densities of residential development. Development in this area should continue to support the Downtown as a whole by the following actions:

- Support a variety of housing types that meet market demand and provide easy access to Downtown uses for a large number of residents (*MCP*)
- Ensure development regulations address the impacts of Downtown uses on residential areas, including parking, access, building design, utility screening, etc. (*LDC*)
- Utilize vacant lots as an opportunity to increase the residential population close to the Downtown (*MCP*)
- Require development to reflect the character of the Downtown area through attractive building design and architectural materials (*LDC*)
- Provide pedestrian connections to the Main Street area and surrounding neighborhoods to support multi-modal access to Downtown (*MCP*)

#### PARKING MANAGEMENT

Parking is an important component in many commercial areas, allowing access to local shops and restaurants. However, the location and configuration of surface parking lots are often detrimental to the desired character of Downtown. In order to balance the goals of providing sufficient parking capacity and maintaining a traditional Downtown character, the City should implement the following strategies:

- Develop or screen existing surface parking lots that front on Main Street or adjacent uses (*MCP*)
- Use alleys for access to parking areas (*MCP*)
- Work with property owners to relocate parking lots that currently front on Main Street to rear portions of the block so that Main Street frontage can be redeveloped with buildings that reinforce the desired character (*MCP*)
- Periodically conduct parking surveys that assess the capacity and location of Downtown parking relative to

demand at different times of the day and week (MCP) (GDD)

- Encourage shared parking agreements between Downtown tenants or for special events (i.e. use of bank parking during weekend events or festivals) (LDC) MCP) (GDD)

(Ord. 2574, Ord. \_\_\_\_)

## MAIN STREET CORRIDOR

The Main Street corridor includes commercial properties along US-56/Main Street from Moonlight Road to Sycamore Street. This corridor serves as the gateway into Gardner, though the fragmented development pattern and surface parking frontage create a commercial environment that lacks cohesion and character.

Recommendations within this section complement those within the US-56 Corridor Management Plan.

### ACCESS MANAGEMENT

Multiple curb cuts and parking lots along the Main Street corridor reduce the efficiency of the roadway network, and result in a high number of conflict points. Access management principles can be used to enhance access to local uses, and support multi-modal mobility. The City should consider the following access management strategies (LDC, MCP):

- Encourage the use of cross-access between adjacent lots that would enhance circulation without reliance on turning movements on Main Street (MCP)
- Prioritize access to parking areas from side streets and alleys rather than from Main Street (MCP)
- Work with property owners to consolidate or remove excessive curb cuts to individual properties
- Review development regulation related to parking to ensure that required on-site capacity does not create undue strain, especially within the context of other regulations related to landscaping, access, required yards, and buffers against other land uses (LDC)
- Allow for and encourage shared parking agreements between uses whose parking demand occurs during different times of the day or week (LDC)

### CHARACTER

As the gateway into Gardner, the character of Main Street plays an important role in Gardner's overall image and identity. Currently, the wide street cross-section and unscreened parking lots are the predominant visual elements of Main Street. Though they are often set back from the street, the design and orientation of buildings on Main Street have a significant impact on the character of Gardner. In order to foster a vibrant commercial corridor, and have Main Street serve as a positive entrance into Gardner, the City should implement the following strategies:

- Actively enforce municipal ordinances to ensure that structures and landscaping are properly maintained and in compliance with development and building codes
- Review and amend sign regulations as appropriate to require signage that is attractive, integrated into building and landscape design, and consistent with the desired character of the Main Street corridor (LDC)
- Work with property owners to install parking edge landscaping and trees to decrease the visual impacts of surface parking lots
- Work with KDOT to assess the viability of, and implement a reconfigured Main Street that would maintain traffic flow, enhance vehicular safety, and create additional space for sidewalks, bicycle lanes, and/or landscaping areas (MCP) (GDD)

### INVESTMENT & REDEVELOPMENT

Redevelopment along Main Street is inhibited by small commercial lots and a close relationship to surrounding residential areas. In order to appropriately manage this relationship and maximize opportunities for investment and redevelopment, the City should implement the following actions:

- Support investment in existing commercial structures and sites through the development of a façade enhancement program, business improvement district, the pursuit of outside funding, or other policy and funding alternatives (MCP)
- Identify areas where commercial development on the north side of Main Street can encroach north, to Shawnee Street, by redeveloping compromised or vacant residential structures, and amending zoning regulations to require significant screening where the future commercial development may back up to existing residential lots (MCP)

- Work with property owners and developers to identify opportunities to assemble lots to create more substantial redevelopment sites that can better accommodate larger building footprints, on-site parking and circulation, landscaping, and required yards (MCP)

(Ord. 2574, Ord. \_\_\_\_)

## CHAPTER 9. IMAGE & IDENTITY

### STREETSCAPE & BUILT FORM

There are several portions of Gardner where the character of the built environment reflects the traditional heritage of the community. This is the result of efforts to preserve important structures and invest in the public realm through streetscape and civic spaces. The design of private development – building placement, materials, architectural massing and detail, landscaping, and signage – can be used to create a sense of enclosure for the public realm, and can create an attractive and inviting environment. Streetscape elements such as lighting, landscaping, street furniture, and sidewalk design can be used to create a unique sense of place. In order to maximize the benefits of investment in private development and public spaces, the City should implement the following actions:

- Continue to implement the Design Standards of the *Gardner Land Development Code* through development review and approval.
- Establish a comprehensive streetscape master plan that identifies priority streetscape corridors, illustrates desired streetscape elements, includes standards for design and installation, and considers opportunities to collaborate with private development to extend the streetscape beyond the public realm. This streetscape master plan should include recommendations and illustrations related to typical streetscape elements that are appropriate for various environments, civic spaces, gateways, and wayfinding. (MCP) (GDD)
- Install gateways at prominent entry points to Gardner. The I-35 interchanges and several at-grade streets represent the primary means of entry into Gardner. The City should commission the design and installation of substantial gateway markers and entry points from US 56 and Gardner Road, while more modest gateway elements could announce entry. (MCP)
- Use streetscape design as a way to call attention to Gardner's varying commercial environments. Gardner's commercial areas vary in terms of function, level of visibility, and intended character. For example, the US-56/I-35 interchange area is auto-oriented and could include large areas of landscaping and decorative detention. However, the Downtown is pedestrian-oriented and would benefit from safe sidewalk zones, buffers from vehicular traffic, sidewalk café seating and street furniture that encourages strolling and staying. The City should utilize streetscape to call attention to and differentiate these important areas. Contrasting lighting elements, landscaping, signage, and hardscape can create this effect. (GDD)
- Place attractive wayfinding signs in commercial areas and along major corridors. Signs should be located in key commercial locations in the City, such as Main Street, as well as along arterial corridors to direct motorists and pedestrians to destinations such as City Hall, the Johnson County Fairgrounds, and Celebration Park. The scale of the signs should be appropriately sized based on location, but all should have a similar design to unify and define the City's brand and identity. (MCP) (GDD)

(Ord. 2574, Ord. \_\_\_\_)

## CHAPTER 10. SUSTAINABILITY FRAMEWORK

The term "sustainability" can have several meanings depending on local context and priorities. Generally, sustainability includes consideration of three important elements in urban development; local ecology, economic well-being, and social equity. In Gardner, the balance among these three elements is influenced by oil drilling, intermodal distribution and logistics, traditional and contemporary neighborhood development, bicycle trails and greenways, access to basic goods and services, and many other factors. As these urban development issues evolve, the City is also subject to changing climate conditions that will continue to test local infrastructure, energy systems, and consumer behavior.

This chapter highlights recommendations throughout the Comprehensive Plan that reflect sustainable practices, and introduces additional strategies aimed at ensuring Gardner's long-term viability and attractiveness. Additional recommendations for sustainable practices and quality of life improvements can be found in the *Gardner Main Street Corridor Plan*, adopted by reference as part of the Comprehensive Plan in 2018 (Ord. 2574), and in the *Gardner Destination Downtown: A Placemaking and Mobility Enhancement Project plan*, adopted by reference as part of the Comprehensive Plan in 2020 (Ord. \_\_\_\_).



## CHAPTER 11. IMPLEMENTATION

### SHORT-TERM TASKS (0-3 YEARS)

#### CREATE AN IMPLEMENTATION ACTION AGENDA

The Comprehensive Plan provides more than 200 policy and program recommendations. In order to address the myriad of strategies presented within the plan, the City should prepare an “Action Agenda,” to highlight the projects and activities to be undertaken within the next few years. The action agenda should be developed in conjunction with the City’s CIP, and should involve all relevant City departments. To remain current, the action agenda should be updated once a year. The action agenda may consist of:

- A detailed description of the projects and activities to be undertaken;
- The priority of each project or activity;
- An indication of the public and private sector responsibilities for initiating and participating in each activity; and
- Potential funding sources and assistance programs that might be available for implementing each project or activity.

A table describing all the recommendations of the 2014 Comprehensive Plan, as amended, is included as Appendix A. It is intended that this table be updated yearly. (Ord. 2554) This action plan in Appendix A does not include the recommendations of the *Gardner Main Street Corridor Plan*, *Gardner Destination Downtown Plan*, or other plans adopted by reference as part of this plan, which should also be considered when preparing the City’s CIP and when updating the Capital Improvements Element (CIE) of Chapter 12. A similar implementation table referencing all recommendations of the *Gardner Main Street Corridor Plan* is contained in Chapter 7 of that document, and in Chapter 5 of the *Gardner Destination Downtown Plan* (Ord. 2574, Ord. \_\_\_\_)

#### DEVELOP SPECIFIC PLANS FOR PRIORITY DEVELOPMENT AREAS

The Comprehensive Plan provides several recommendations for the creation of more specific and detailed plans throughout Gardner. The City should initiate the process to further study and create plans for the following areas:

- Downtown
- Johnson County Fairgrounds
- I-35 Interchange Areas
- 191st Street Corridor
- Main Street Corridor
- Southeast Quadrant Market-Determined Area

In 2016, the *I-35 & Gardner Road Interchange Subarea Plan* was adopted, providing further insight for the western portion of the Southeast Quadrant Market-Determined Area (including the I-35 and Gardner Road Interchange and the 191<sup>st</sup> Street Corridor).

In 2018, the *Gardner Main Street Corridor Plan* was adopted as part of a grant-supported initiative to further study and recommend improvements for the Downtown, Johnson County Fairgrounds, Main Street corridor, surrounding neighborhoods, and the area adjacent to the I-35 and 175<sup>th</sup> Street interchange.

In 2018, the *I-35 & 175<sup>th</sup> Street Interchange Subarea Plan* was adopted, providing further insight for the northern portion of the Southeast Quadrant Growth Areas (including the I-35 and 175<sup>th</sup> Street Interchange).

In 2020, the *Gardner Destination Downtown: A Placemaking and Mobility Enhancement Project* plan was adopted as part of a grant-supported initiative to support more detailed planning for new open and civic spaces and improved streetscapes within an 8-block area of Downtown between Center Street on the west, Sycamore Street on the east, E Washington Street on the north, and E Warren Street on the south (including Cornerstone Park on E Washington Street).

Although these area plans are separate documents, incorporated by reference into the Comprehensive Plan, these updated plans establish the official Comprehensive Plan policy for the respective study areas. The recommendations in these referenced plans take precedence over other policy found in other portions of the Comprehensive Plan, unless otherwise

noted. However, applicable recommendations of the Comprehensive Plan should be considered in addition to the recommendations of these respective area plans.

(Ord. 2574, Ord. 2582, Ord. \_\_\_\_)

### **MEDIUM-TERM TASKS (3-5 YEARS)**

#### **ADDITIONAL ACTIONS**

In addition to the aforementioned actions, the Plan provides recommendations for several medium-term tasks. These include:

- Conduct a sidewalk gap analysis
- Conduct a Gardner Golf Course reuse and study plan
- Implement Safe Routes to School projects
- Adopt Complete Streets policy – such policy was adopted as Appendix B of the *Gardner Land Development Code*, effective August 1, 2016.
- Create a Downtown-specific plan that addresses detailed land use, revitalization, and streetscape improvements – see the *Gardner Main Street Corridor Plan* and *Gardner Destination Downtown plan* adopted by reference as part of this plan.
- Work with the Johnson County Fair Association to identify potential locations for a new facility within the Gardner community (see also the public input and findings in the *Gardner Main Street Corridor Plan*)

(Ord. 2554, Ord. 2574, Ord. \_\_\_\_)

### **LONG-TERM TASKS (5+ YEARS)**

#### **RECONFIGURE MAIN STREET**

The redesign and streetscape improvements of Main Street/US-56 to create a pedestrian and business-friendly environment is paramount to the revitalization of Downtown. It will require additional studies and design concepts and the potential transfer of roadway jurisdiction from the Kansas Department of Transportation to the City of Gardner. The *Gardner Main Street Corridor Plan*, adopted by reference as part of this plan, contains additional studies and design concepts for Main Street, and recommends against the transfer of roadway jurisdiction from KDOT to Gardner. (Ord. 2574) *The Gardner Destination Downtown plan*, adopted by reference as part of this plan, contains very specific recommendations for not only redesign and streetscape improvements for Main Street, but for E Washington, E Shawnee, E Park, E Warren, Sycamore, and Elm Streets. It also provides concept plans and implementation strategies for new public spaces near City Hall, improvements in Cornerstone Park, and a mobility hub on Park Street. This plan provides sufficient detail to solicit design and construction documents for Downtown improvements, moving the City closer to realizing this task. (Ord. \_\_\_\_)

Planning Commission  
RESOLUTION NO. PC-20-01

A RESOLUTION ADOPTING AMENDMENTS TO THE COMPREHENSIVE PLAN FOR THE CITY OF GARDNER, KANSAS, AMENDING THE COVER PAGE, CHAPTER 1 INTRODUCTION, CHAPTER 3 PUBLIC PARTICIPATION, CHAPTER 4 VISION, GOALS & OBJECTIVES, CHAPTER 5 LAND USE & DEVELOPMENT, CHAPTER 9 IMAGE & IDENTITY, CHAPTER 10 SUSTAINABILITY FRAMEWORK, CHAPTER 11 IMPLEMENTATION AND INCORPORATING BY REFERENCE THE GARDNER DESTINATION DOWNTOWN: A PLACEMAKING AND MOBILITY ENHANCEMENT PROJECT PLAN

WHEREAS, the City of Gardner has a duly constituted Planning Commission as provided by K.S.A 12-744; and

WHEREAS, pursuant to K.S.A. 12-747(a), the Gardner Planning Commission is authorized to make or cause to be made a comprehensive plan for the development of the City of Gardner and any unincorporated territory lying outside of the City but within the same County in which the City is located, which in the opinion of the Planning Commission, forms the total community of which the City is a part; and

WHEREAS, pursuant to K.S.A. 12-747(b), the Gardner Planning Commission is authorized to make recommendations to the Governing Body of the City relating to the adoption and amendment of a comprehensive plan by a single resolution or by successive resolutions and is further authorized to adopt or amend parts of the comprehensive plan for the City of Gardner; and

WHEREAS, the City of Gardner adopted a Comprehensive Plan titled "City of Gardner Comprehensive Plan", approved with Ordinance 2457 on September 15, 2014; and amended with Ordinance No. 2517 on June 20, 2016, and Ordinance No. 2553 on September 18, 2017, and Ordinance No. 2554 on September 18, 2017, and Ordinance No. 2574 on April 16, 2018, and Ordinance 2582 on June 18, 2018; and

WHEREAS, the proposed Amendments to the Comprehensive Plan for the City of Gardner, Kansas amending the Cover Page, Chapter 1 Introduction, Chapter 3 Public Participation, Chapter 4 Vision, Goals & Objectives, Chapter 5 Land Use & Development, Chapter 9 Image & Identity, Chapter 10 Sustainability Framework, and Chapter 11 Implementation including the *Gardner Destination Downtown: A Placemaking and Mobility Enhancement Project* plan was prepared for review by the Gardner Planning Commission in accordance with K.S.A. 12-747, and

WHEREAS, proper notice was published in the official City newspaper once at least twenty (20) days prior to the public hearing on the proposed amendment to the City of Gardner Comprehensive Plan; and

WHEREAS, the Planning Commission held a public hearing on the proposed Amendments to the Comprehensive Plan for the City of Gardner, Kansas amending the Cover



Page, Chapter 1 Introduction, Chapter 3 Public Participation, Chapter 4 Vision, Goals & Objectives, Chapter 5 Land Use & Development, Chapter 9 Image & Identity, Chapter 10 Sustainability Framework, and Chapter 11 Implementation including the *Gardner Destination Downtown: A Placemaking and Mobility Enhancement Project* plan on February 25, 2020, at which public comments were encouraged; and

WHEREAS, the Gardner Planning Commission is recommending to the Governing Body the Amendments to the Comprehensive Plan for the City of Gardner, Kansas amending the Cover Page, Chapter 1 Introduction, Chapter 3 Public Participation, Chapter 4 Vision, Goals & Objectives, Chapter 5 Land Use & Development, Chapter 9 Image & Identity, Chapter 10 Sustainability Framework, and Chapter 11 Implementation and adopting and incorporating by reference the *Gardner Destination Downtown: A Placemaking and Mobility Enhancement Project* plan, in accordance with K.S.A. 12-747

NOW, THEREFORE, BE IT RESOLVED, that the Planning Commission of the City of Gardner, Kansas, hereby adopts the proposed Amendments to the Comprehensive Plan for the City of Gardner, Kansas, incorporated herein by reference, amending the Cover Page, Chapter 1 Introduction, Chapter 3 Public Participation, Chapter 4 Vision, Goals & Objectives, Chapter 5 Land Use & Development, Chapter 9 Image & Identity, Chapter 10 Sustainability Framework, Chapter 11 Implementation and the *Gardner Destination Downtown: A Placemaking and Mobility Enhancement Project* plan, and recommends adoption of same of by the Governing Body in accordance with K.S.A. 12-747.

This Resolution, together with a certified copy of the Amendments to the Comprehensive Plan for the City of Gardner, Kansas amending the Cover Page, Chapter 1 Introduction, Chapter 3 Public Participation, Chapter 4 Vision, Goals & Objectives, Chapter 5 Land Use & Development, Chapter 9 Image & Identity, Chapter 10 Sustainability Framework, and Chapter 11 Implementation and the *Gardner Destination Downtown: A Placemaking and Mobility Enhancement Project* plan, and a written summary of the Planning Commission Public Hearing conducted regarding said Amendments, shall be submitted to the Governing Body pursuant to K.S.A. 12-747.

ADOPTED by the Planning Commission of the City of Gardner, Kansas this 25th day of February, 2020.

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Scott Boden, Chairman

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Larry J. Powell, Secretary